



County of Fresno

DEPARTMENT OF PUBLIC WORKS AND PLANNING
STEVEN E. WHITE, DIRECTOR

DATE: November 22, 2019

TO: Development Services and Capital Projects, Attn: William M. Kettler, Division Manager
Development Services and Capital Projects, Attn: Chris Motta, Principal Planner
Development Services and Capital Projects, Current Planning, Attn: Marianne Mollring, Senior Planner
Development Services and Capital Projects, Policy Planning, ALCC, Attn: Mohammad Khorsand
Development Services and Capital Projects, Zoning & Permit Review, Attn: Daniel Gutierrez
Development Services and Capital Projects, Site Plan Review, Attn: Hector Luna
Development Services and Capital Projects, Building & Safety/Plan Check, Attn: Chuck Jonas
Development Engineering, Attn: Laurie Kennedy, Grading/Mapping
Road Maintenance and Operations, Attn: John Thompson/Wendy Nakagawa/Nadia Lopez
Design Division, Transportation Planning, Attn: Mohammad Alimi/Dale Siemer/Brian Spaunhurst
Resources Division/Special Districts, Attn: Amina Flores-Becker, Daniel Vang
Water and Natural Resources Division, Attn: Glenn Allen, Division Manager
Department of Public Health, Environmental Health Division, Attn: Deep Sidhu/Steven Rhodes
Agricultural Commissioner, Attn: Melissa Cregan
U.S. Fish and Wildlife Service, San Joaquin Valley Division, Attn: Patricia Cole, Matthew Nelson
CA Regional Water Quality Control Board, Attn: Dale Harvey
Fresno County Sheriffs Department, Attn: John Zanoni, John Reynolds, Louis Hernandez, Kathy Curtis, Ryan Hushaw
CA Department of Fish and Wildlife, Attn: Renee Robison, Environmental Scientist
California Department of Transportation, District 6, Attn: David Padilla
Dumna Wo Wah Tribal Government, Attn: Robert Ledger, Tribal Chairperson
Picayune Rancheria of the Chukchansi Indians, Attn: Heather Airey, THPO/Cultural Resources Director
Table Mountain Rancheria, Attn: Leanne Walker-Grant, Tribal Chairperson
Robert Pennell, Cultural Resources Director/Kim Taylor, Cultural Resources Department/Sara Barnett, Cultural Resources Department
Department
Santa Rosa Rancheria Tachi-Yokut Tribe, Attn: Hector Franco, Director/Shana Powers, Cultural Specialist
San Joaquin Valley Unified Air Pollution Control District (PIC-CEQA Division), Attn: PIC Supervisor
Sierra Resource Conservation District, Attn: Steve Haze
Fresno County Fire Protection District, Attn: Jim McDougald, Division Chief
U.S. Army Corps of Engineers, Sacramento District, Regulatory Division, CA South Branch, Attn: Kathy Norton

U.S. Forest Service, Sierra National Forest, Attn: Dean A. Gould, Antonio Cabrera

FROM: Jeremy Shaw, Planner JS
Development Services and Capital Projects Division

SUBJECT: Initial Study Application No. 7593, Unclassified Conditional Use Permit Application
No. 3639 (Amending CUP 3487)

APPLICANT: Pascual Garcia/Southern California Edison

DUE DATE: December 9, 2019

The Department of Public Works and Planning, Development Services and Capital Projects Division has prepared an Initial Study for the subject application proposing to allow the expansion of an existing Southern California Edison Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution system on a 2.62-acre portion of a 357.80-acre parcel in the RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District. The project site is located on the north side of Dinkey Creek Road, approximately one quarter-mile east of State Route 168/Tollhouse Road, within the unincorporated community of Shaver Lake (SUP. DIST. 5) (APN 120-260-10U) (41694 Dinkey Creek Road).

Please review the documents at: www.co.fresno.ca.us/InitialStudies

If you have any comments regarding the adequacy and/or accuracy of the Initial Study document, please provide comments to my attention prior to the deadline. We must have your comments by December 9, 2019. Any comments received after this date may not be used.

NOTE - If you do not have comments, please provide a "no comment" response to our office by the above deadline (e-mail is also acceptable, see address below).

Please address any correspondence or questions to me, Jeremy Shaw, Current Planning Unit, Development Services Division, Fresno County Department of Public Works and Planning, 2220 Tulare Street, Sixth Floor, Fresno, CA 93721, or call (559) 600-4207, or email jshaw@fresnocountyca.gov

We must have your comments by December 9, 2019. Any comments received after this date may not be used.

NOTE - THIS WILL BE OUR ONLY REQUEST FOR WRITTEN COMMENTS. If you do not have comments, please provide a "NO COMMENT" response to our office by the above deadline (e-mail is also acceptable; see email address below).

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Activity Code (Internal Review):2384

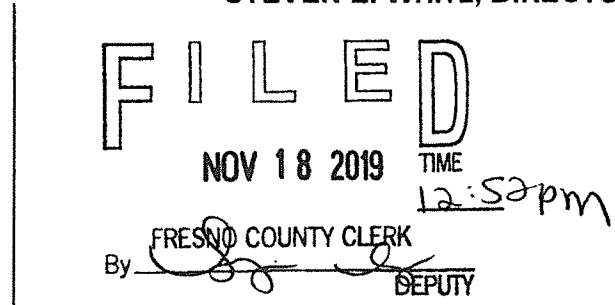


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County of Fresno

DEPARTMENT OF PUBLIC WORKS AND PLANNING
STEVEN E. WHITE, DIRECTOR

NOTICE OF INTENT TO ADOPT A MITIGATED NEGATIVE DECLARATION



For County Clerk's Stamp

Notice is hereby given that the County of Fresno has prepared Initial Study Application (IS) No. 7593 pursuant to the requirements of the California Environmental Quality Act for the following proposed project:

INITIAL STUDY APPLICATION NO. 7593 and UNCLASSIFIED CONDITIONAL USE PERMIT (CUP) APPLICATION NO. 3639 filed by **PASCUAL GARCIA (SOUTHERN CALIFORNIA EDISON)**, proposing to allow the expansion of an existing Southern California Edison Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution system on a 2.62-acre portion of a 357.80-acre parcel in the RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District. The subject parcel is located on the north side of Dinkey Creek Road, approximately one quarter-mile east of State Route 168/Tollhouse Road, within the unincorporated community of Shaver Lake (41694 Dinkey Creek Road) (SUP. DIST. 5) (APN 120-260-10U).

(hereafter, the "Proposed Project")

The County of Fresno has determined that it is appropriate to adopt a Mitigated Negative Declaration for the Proposed Project. The purpose of this Notice is to (1) provide notice of the availability of IS Application No. 7593 and the draft Mitigated Negative Declaration, and request written comments thereon; and (2) provide notice of the public hearing regarding the Proposed Project.

Public Comment Period

The County of Fresno will receive written comments on the Proposed Project and Mitigated Negative Declaration from November 20, 2019 through December 19, 2019.

Email written comments to jshaw@fresnocountyca.gov, or mail comments to:

Fresno County Department of Public Works and Planning
Development Services and Capital Projects Division
Attn: Jeremy Shaw
2220 Tulare Street, Suite A
Fresno, CA 93721

IS Application No. 7593 and the draft Mitigated Negative Declaration may be viewed at the above address Monday through Thursday, 9:00 a.m. to 5:00 p.m., and Friday, 8:30 a.m. to 12:30 p.m. (except holidays), or at www.co.fresno.ca.us/initialstudies. An electronic copy of the

DEVELOPMENT SERVICES AND CAPITAL PROJECTS DIVISION
2220 Tulare Street, Sixth Floor / Fresno, California 93721 / Phone (559) 600-4497 / 600-4022 / 600-4540 / FAX 600-4200
The County of Fresno is an Equal Employment Opportunity Employer

draft Mitigated Negative Declaration for the Proposed Project may be obtained from Jeremy Shaw at the addresses above.

Public Hearing

The Planning Commission will hold a public hearing to consider approving the Proposed Project and the Mitigated Negative Declaration on January 9, 2020, at 8:45 a.m., or as soon thereafter as possible, in Room 301, Hall of Records, 2281 Tulare Street, Fresno, California 93721. Interested persons are invited to appear at the hearing and comment on the Proposed Project and draft Mitigated Negative Declaration.

For questions please call Jeremy Shaw (559) 600-4207.

Published: November 20, 2019

Notice of Completion & Environmental Document Transmittal

Mail to: State Clearinghouse, P.O. Box 3044, Sacramento, CA 95812-3044 (916) 445-0613
 For Hand Delivery/Street Address: 1400 Tenth Street, Sacramento, CA 95814

SCH #

Project Title: Initial Study Application No.7593/ Unclassified Conditional Use Permit Application No. 3639

Lead Agency: Fresno County Department of Public Works and Planning Contact Person: Jeremy Shaw
 Mailing Address: 2220 Tulare Street, 6th Floor Phone: (559) 600-4207
 City: Fresno Zip: 93721 County: Fresno

Project Location: County: Fresno City/Nearest Community: Shaver Lake
 Cross Streets: SR 168/Dinkey Creek Road Zip Code: 93664

Longitude/Latitude (degrees, minutes and seconds): 37 ° 06 ' 19.44" N / 119 ° 18 ' 47.59" W Total Acres: 357.80 acres
 Assessor's Parcel No.: 120-260-10U Section: 36 Twp.: 9S Range: 24E Base: MDBM
 Within 2 Miles: State Hwy #: 168 Waterways: N/A
 Airports: N/A Railways: N/A Schools: N/A

Document Type:

CEQA: NOP Draft EIR NEPA: NOI Other: Joint Document
 Early Cons Supplement/Subsequent EIR EA Final Document
 Neg Dec (Prior SCH No.) Draft EIS Other: _____
 Mit Neg Dec Other: _____
 FONSI

Local Action Type:

General Plan Update Specific Plan Rezone Annexation
 General Plan Amendment Master Plan Prezone Redevelopment
 General Plan Element Planned Unit Development Use Permit Coastal Permit
 Community Plan Site Plan Land Division (Subdivision, etc.) Other: _____

Development Type:

Residential: Units _____ Acres _____
 Office: Sq.ft. _____ Acres _____ Employees _____ Transportation: Type _____
 Commercial: Sq.ft. _____ Acres 2.62 Employees _____ Mining: Mineral _____
 Industrial: Sq.ft. _____ Acres _____ Employees _____ Power: Type _____ MW _____
 Educational: _____ Waste Treatment: Type _____ MGD _____
 Recreational: _____ Hazardous Waste: Type _____
 Water Facilities: Type _____ MGD _____ Other: _____

Project Issues Discussed in Document:

Aesthetic/Visual Fiscal Recreation/Parks Vegetation
 Agricultural Land Flood Plain/Flooding Schools/Universities Water Quality
 Air Quality Forest Land/Fire Hazard Septic Systems Water Supply/Groundwater
 Archeological/Historical Geologic/Seismic Sewer Capacity Wetland/Riparian
 Biological Resources Minerals Soil Erosion/Compaction/Grading Growth Inducement
 Coastal Zone Noise Solid Waste Land Use
 Drainage/Absorption Population/Housing Balance Toxic/Hazardous Cumulative Effects
 Economic/Jobs Public Services/Facilities Traffic/Circulation Other: _____

Present Land Use/Zoning/General Plan Designation:

Southern California Edison Shaver Lake Service Center/RC-40/Open Space (in the County adopted Shaver Lake Community Plan)

Project Description: (please use a separate page if necessary)

Allow the expansion of an existing Southern California Edison, Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution system on a 2.62-acre portion of a 357.80-acre parcel in the RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District.

Note: The State Clearinghouse will assign identification numbers for all new projects. If a SCH number already exists for a project (e.g. Notice of Preparation or previous draft document) please fill in.

Reviewing Agencies Checklist

Lead Agencies may recommend State Clearinghouse distribution by marking agencies below with an "X". If you have already sent your document to the agency please denote that with an "S".

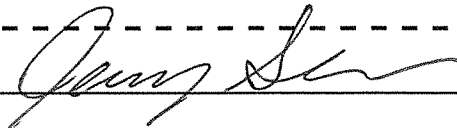
- | | |
|---|--|
| <input checked="" type="checkbox"/> Air Resources Board | <input type="checkbox"/> Office of Historic Preservation |
| <input type="checkbox"/> Boating & Waterways, Department of | <input type="checkbox"/> Office of Public School Construction |
| <input type="checkbox"/> California Emergency Management Agency | <input type="checkbox"/> Parks & Recreation, Department of |
| <input type="checkbox"/> California Highway Patrol | <input type="checkbox"/> Pesticide Regulation, Department of |
| <input type="checkbox"/> Caltrans District # _____ | <input type="checkbox"/> Public Utilities Commission |
| <input type="checkbox"/> Caltrans Division of Aeronautics | <input checked="" type="checkbox"/> Regional WQCB # _____ |
| <input type="checkbox"/> Caltrans Planning | <input type="checkbox"/> Resources Agency |
| <input type="checkbox"/> Central Valley Flood Protection Board | <input type="checkbox"/> Resources Recycling and Recovery, Department of |
| <input type="checkbox"/> Coachella Valley Mtns. Conservancy | <input type="checkbox"/> S.F. Bay Conservation & Development Comm. |
| <input type="checkbox"/> Coastal Commission | <input type="checkbox"/> San Gabriel & Lower L.A. Rivers & Mtns. Conservancy |
| <input type="checkbox"/> Colorado River Board | <input type="checkbox"/> San Joaquin River Conservancy |
| <input type="checkbox"/> Conservation, Department of | <input type="checkbox"/> Santa Monica Mtns. Conservancy |
| <input type="checkbox"/> Corrections, Department of | <input type="checkbox"/> State Lands Commission |
| <input type="checkbox"/> Delta Protection Commission | <input type="checkbox"/> SWRCB: Clean Water Grants |
| <input type="checkbox"/> Education, Department of | <input checked="" type="checkbox"/> SWRCB: Water Quality |
| <input type="checkbox"/> Energy Commission | <input type="checkbox"/> SWRCB: Water Rights |
| <input checked="" type="checkbox"/> Fish & Game Region # _____ | <input type="checkbox"/> Tahoe Regional Planning Agency |
| <input type="checkbox"/> Food & Agriculture, Department of | <input type="checkbox"/> Toxic Substances Control, Department of |
| <input checked="" type="checkbox"/> Forestry and Fire Protection, Department of | <input type="checkbox"/> Water Resources, Department of |
| <input type="checkbox"/> General Services, Department of | <input type="checkbox"/> Other: _____ |
| <input checked="" type="checkbox"/> Health Services, Department of | <input type="checkbox"/> Other: _____ |
| <input type="checkbox"/> Housing & Community Development | |
| <input checked="" type="checkbox"/> Native American Heritage Commission | |

Local Public Review Period (to be filled in by lead agency)

Starting Date November 20, 2019 Ending Date December 19, 2019

Lead Agency (Complete if applicable):

Consulting Firm: _____	Applicant: <u>Pascual Garcia/ Southern California Edison</u>
Address: _____	Address: <u>800 W. Cienega Avenue</u>
City/State/Zip: _____	City/State/Zip: <u>San Dimas, CA 91773</u>
Contact: _____	Phone: <u>(909) 394-2876</u>
Phone: _____	

Signature of Lead Agency Representative:  Date: 11-18-19

Authority cited: Section 21083, Public Resources Code. Reference: Section 21161, Public Resources Code.



County of Fresno

DEPARTMENT OF PUBLIC WORKS AND PLANNING
STEVEN E. WHITE, DIRECTOR

INITIAL STUDY ENVIRONMENTAL CHECKLIST FORM

1. **Project title:**
Initial Study Application No. 7593, Unclassified Conditional Use Permit No. 3639
2. **Lead agency name and address:**
Fresno County Department of Public Works and Planning
Development Services and Capital Projects Division
2220 Tulare Street, 6th Floor
Fresno, CA 93721-2104
3. **Contact person and phone number:**
Jeremy Shaw, Planner, (559) 600-4207
4. **Project location:**
The subject parcel is located on the north side of Dinkey Creek Road, approximately one quarter-mile east of State Route 168/Tollhouse Road, within the unincorporated community of Shaver Lake (APN 120-260-10U) (SUP. DIST. 5) (41694 Dinkey Creek Road).
5. **Project sponsor's name and address:**
Pascual Garcia/Southern California Edison
800 West Cienega Avenue
San Dimas, CA 91773
6. **General Plan designation:**
The subject parcel is designated as Open Space in the Shaver Lake Community Plan.
7. **Zoning:**
RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District
8. **Description of project: (Describe the whole action involved, including, but not limited to, later phases of the project, and any secondary, support, or off-site features necessary for its implementation. Attach additional sheets if necessary.)**
Allow the expansion of an existing Southern California Edison Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution system on a 2.62-acre portion of a 357.80-acre parcel in the RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District.
9. **Surrounding land uses and setting: Briefly describe the project's surroundings:**
The subject parcel is located in a mountainous area, adjacent to Shaver Lake. The immediate vicinity of the project site contains a mix of residential and commercial land uses.
10. **Other public agencies whose approval is required (e.g., permits, financing approval, or participation agreement.)**
None.
11. **Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code Section 21080.3.1? If so, is there a plan for consultation that includes, for example, the determination of significance of impacts to tribal cultural resources, procedures regarding confidentiality, etc.?**

Under the provisions of Assembly Bill (AB) 52, the County was required to provide notice of preparation of this Initial Study to Native American tribes who had previously indicated interest in reviewing CEQA projects. Notices were sent on April 30, 2019 to the appropriate Tribal government representatives. None of the Tribes requested consultation on or expressed interest in this project.

NOTE: Conducting consultation early in the CEQA process allows tribal governments, lead agencies, and project proponents to discuss the level of environmental review, identify and address potential adverse impacts to tribal cultural resources, and reduce the potential for delay and conflict in the environmental review process. (See Public Resources Code Section 21080.3.2.) Information may also be available from the California Native American Heritage Commission's Sacred Lands File per Public Resources Code Section 5097.96 and the California Historical Resources Information System administered by the California Office of Historic Preservation. Please also note that Public Resources Code Section 21082.3(c) contains provisions specific to confidentiality.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | |
|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture and Forestry Resources |
| <input type="checkbox"/> Air Quality | <input type="checkbox"/> Biological Resources |
| <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Energy |
| <input type="checkbox"/> Geology/Soils | <input type="checkbox"/> Greenhouse Gas Emissions |
| <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology/Water Quality |
| <input type="checkbox"/> Land Use/Planning | <input type="checkbox"/> Mineral Resources |
| <input type="checkbox"/> Noise | <input type="checkbox"/> Population/Housing |
| <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Tribal Cultural Resources |
| <input type="checkbox"/> Utilities/Service Systems | <input type="checkbox"/> Wildfire |
| <input type="checkbox"/> Mandatory Findings of Significance | |

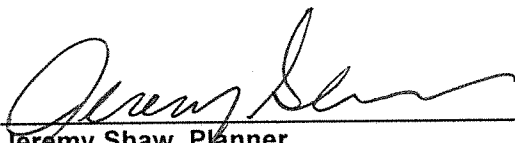
DETERMINATION OF REQUIRED ENVIRONMENTAL DOCUMENT:

On the basis of this initial evaluation:

- I find that the proposed project **COULD NOT** have a significant effect on the environment. **A NEGATIVE DECLARATION WILL BE PREPARED.**
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because the Mitigation Measures described on the attached sheet have been added to the project. **A MITIGATED NEGATIVE DECLARATION WILL BE PREPARED.**
- I find the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required
- I find that as a result of the proposed project, no new effects could occur, or new Mitigation Measures would be required that have not been addressed within the scope of a previous Environmental Impact Report.

PERFORMED BY:

REVIEWED BY:



Jeremy Shaw, Planner



Marianne Mollring, Senior Planner

Date: 11-18-19

Date: 11-18-19

**INITIAL STUDY
ENVIRONMENTAL CHECKLIST FORM
(Initial Study Application No. 7593 and
Unclassified Conditional Use Permit
Application No. 3639)**

The following checklist is used to determine if the proposed project could potentially have a significant effect on the environment. Explanations and information regarding each question follow the checklist.

1 = No Impact

2 = Less Than Significant Impact

3 = Less Than Significant Impact with Mitigation Incorporated

4 = Potentially Significant Impact

I. AESTHETICS

Except as provided in Public Resources Code Section 21099, would the project:

- 2 a) Have a substantial adverse effect on a scenic vista?
- 2 b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?
- 2 c) In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from a publicly accessible vantage point.) If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?
- 3 d) Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

II. AGRICULTURAL AND FORESTRY RESOURCES

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology in Forest Protocols adopted by the California Air Resources Board. Would the project:

- 1 a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?
- 1 b) Conflict with existing zoning for agricultural use, or a Williamson Act Contract?
- 2 c) Conflict with existing zoning for forest land, timberland or timberland zoned Timberland Production?
- 2 d) Result in the loss of forest land or conversion of forest land to non-forest use?
- 2 e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or conversion of forest land to non-forest use?

III. AIR QUALITY

Where available, the significance criteria established by the applicable air quality management district or air pollution control district may be relied upon to make the following determinations. Would the project:

- 2 a) Conflict with or obstruct implementation of the applicable Air Quality Plan?
- 2 b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?
- 2 c) Expose sensitive receptors to substantial pollutant concentrations?
- 2 d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

IV. BIOLOGICAL RESOURCES

Would the project:

- 2 a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?
- 2 b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?
- 1 c) Have a substantial adverse effect on state or federally-protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?
- 2 d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?
- 1 e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?
- 1 f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state Habitat Conservation Plan?

V. CULTURAL RESOURCES

Would the project:

- 3 a) Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?
- 3 b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?
- 3 c) Disturb any human remains, including those interred outside of formal cemeteries?

VI. ENERGY

Would the project:

- 2 a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources during project construction or operation?
- 2 b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

VII. GEOLOGY AND SOILS

Would the project:

- 1 a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:
 - 1 i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault?
 - 1 ii) Strong seismic ground shaking?
 - 1 iii) Seismic-related ground failure, including liquefaction?
 - 1 iv) Landslides?
- 2 b) Result in substantial soil erosion or loss of topsoil?
- 1 c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?
- 1 d) Be located on expansive soil as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?
- 2 e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?
- 1 f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

VIII. GREENHOUSE GAS EMISSIONS

Would the project:

- 2 a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?
- 2 b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

IX. HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- 2 a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?
- 2 b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?
- 1 c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?
- 1 d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, create a significant hazard to the public or the environment?
- 1 e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, result in a safety hazard or excessive noise for people residing or working in the project area?
- 1 f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?
- 2 g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

X. HYDROLOGY AND WATER QUALITY

Would the project:

- 1 a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?
- 1 b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?
- 2 c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would result in substantial erosion or siltation on or off site?
 - 2 i) Result in substantial erosion or siltation on or off site;
 - 2 ii) Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on or off site;
 - 2 iii) Create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff; or
 - 2 iv) Impede or redirect flood flows?
- 1 d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?
- 1 e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

XI. LAND USE AND PLANNING

Would the project:

- 1 a) Physically divide an established community?
- 1 b) Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

XII. MINERAL RESOURCES

Would the project:

- 1 a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?
- 1 b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local General Plan, Specific Plan or other land use plan?

XIII. NOISE

Would the project result in:

- 2 a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?
- 2 b) Generation of excessive ground-borne vibration or ground-borne noise levels?
- 1 c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, exposing people residing or working in the project area to excessive noise levels?

XIV. POPULATION AND HOUSING

Would the project:

- 1 a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and

businesses) or indirectly (for example, through extension of roads or other infrastructure)?

- 1 b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

XV. PUBLIC SERVICES

Would the project:

- 1 a) Result in substantial adverse physical impacts associated with the provision of new or physically-altered governmental facilities, or the need for new or physically-altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:
- 1 i) Fire protection?
- 1 ii) Police protection?
- 1 iii) Schools?
- 1 iv) Parks?
- 1 v) Other public facilities?

XVI. RECREATION

Would the project:

- 1 a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?
- 1 b) Include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?

XVII. TRANSPORTATION

Would the project:

- 1 a) Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?
- 1 b) Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?
- 1 c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?
- 1 d) Result in inadequate emergency access?

XVIII. TRIBAL CULTURAL RESOURCES

Would the project:

- 3 a) Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code Section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:
- 3 i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k), or
- 3 ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1? In applying the criteria set

forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.)

XIX. UTILITIES AND SERVICE SYSTEMS

Would the project:

- 1 a) Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?
- 2 b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?
- 2 c) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?
- 1 d) Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?
- 1 e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

XX. WILDFIRE

If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

- 1 a) Substantially impair an adopted emergency response plan or emergency evacuation plan?
- 2 b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?
- 2 c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?
- 2 d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

XXI. MANDATORY FINDINGS OF SIGNIFICANCE

Would the project:

- 3 a) Have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?
- 1 b) Have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)
- 1 c) Have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Documents Referenced:

This Initial Study is referenced by the documents listed below. These documents are available for public review at the County of Fresno, Department of Public Works and Planning, Development Services and Capital Projects Division, 2220 Tulare Street, Suite A, Fresno, California (corner of M & Tulare Streets).

- Fresno County General Plan, Policy Document and Final EIR
- Fresno County General Plan Background Report
- Fresno County Zoning Ordinance
- Important Farmland 2016 Map, State Department of Conservation

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County of Fresno

DEPARTMENT OF PUBLIC WORKS AND PLANNING
STEVEN E. WHITE, DIRECTOR

EVALUATION OF ENVIRONMENTAL IMPACTS

- APPLICANT: Pascual Garcia (Southern California Edison)
- APPLICATION NOS.: Initial Study Application No. 7593 and Unclassified Conditional Use Permit Application No. 3639
- DESCRIPTION: Allow the expansion of an existing Southern California Edison Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution system on a 2.62-acre portion of a 357.80-acre parcel in the RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District.
- LOCATION: The project site is located on the north side of Dinkey Creek Road, approximately one quarter-mile east of State Route 168/Tollhouse Road, within the unincorporated community of Shaver Lake (SUP. DIST. 5) (APN 120-260-10U) (41694 Dinkey Creek Road).

I. AESTHETICS

Except as provided in Public Resources Code Section 21099, would the project:

- A. Have a substantial adverse effect on a scenic vista; or
- B. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project site is located northerly adjacent to Dinkey Creek Road which is designated as a scenic drive in the Open Space and Conservation Element of the Fresno County General Plan. However, the proposed development will not take access directly from Dinkey Creek Road; rather, the project site will be accessed via a private road connecting to Dinkey Creek Road. Additionally, the project area will be partially screened from the roadway by existing trees.

The proposed laydown storage yard will be surrounded by an eight-foot-tall chain-link fence, topped with barbed wire; additionally, the nearest point of the proposed storage yard will be located approximately 93 feet from the nearest right-of-way of Dinkey Creek Road.

Access to the proposed laydown yard will be via a proposed new paved access driveway connecting with the existing paved private road. The construction of the access driveway will involve the removal of some granite outcroppings and trees to allow for the grading and construction of the driveway; however, as the project involves minimal removal of trees and rocks, and grading of the site, there would be less than significant impacts to scenic vistas or scenic resources.

- C. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage points.) If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The proposed project site encompasses a 2.62-acre portion of the subject parcel, which will be added to the existing Shaver Lake Service Center, which includes an 11,842 square-foot administration main building, within an approximately 2.0-acre fenced area with various accessory buildings, fuel pumps with underground storage tanks, and an above-ground propane storage tank.

The proposed laydown yard will provide for the storage of material and equipment for the construction and maintenance of Southern California Edison's (SCE's) transmission and distribution infrastructure. Construction material will be delivered to the site before it is redistributed to the other construction sites. This proposal also entails the installation of two new 1,440 square-foot mobile office trailers.

Dinkey Creek Road, from which the service center takes access, is designated as a scenic drive in the Fresno County General Plan, Open Space and Conservation Element. The project site would take access from an existing paved private road connecting to Dinkey Creek Road, and no new access drives from Dinkey Creek Road will be added.

The project entails the expansion of an existing facility which is accessed by the public; however, no public view points were identified in the analysis, thus the project will not substantially degrade the existing visual character or quality of public views of the site and its surroundings. The project is not located in an urbanized area.

- D. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?

FINDING: LESS THAN SIGNIFICANT IMPACT WITH MITIGATION INCORPORATED:

Review of the applicant's operational statement indicates that there will be one approximately 25-foot-tall pole mounted with security lighting. Accordingly, the following Mitigation Measure will be included to address proposed or existing lighting.

* **Mitigation Measure**

1. *All outdoor lighting shall be hooded and directed downward to avoid shining on adjacent property or the public roadway.*

II. AGRICULTURAL AND FORESTRY RESOURCES

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology in Forest Protocols adopted by the California Air Resources Board. Would the project:

- A. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use; or

- B. Conflict with existing zoning for agricultural use, or a Williamson Act Contract?

FINDING: NO IMPACT:

The subject property is not restricted under Williamson Act contract.

- C. Conflict with existing zoning for forestland, timberland or timberland zoned Timberland Production?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The subject property is not zoned as Timberland Preserve; however, the growing and harvesting of timber and forest products, as well as uses and facilities appurtenant to timber growing and harvesting, are allowed uses in the Resource Conservation Zone District.

The proposed project entails the minor expansion of an existing facility, which would involve the clearing of trees, and grading and paving of approximately 2.62 acres of the 357.80-acre parcel, to be utilized for the storage of materials and equipment used in the maintenance of existing Public Utilities infrastructure.

The nearest Timberland Preserve-zoned area is located approximately two miles east of the subject parcel; thus, the proposed project will not conflict with land zoned for Timber production or forest land.

- D. Result in the loss of forest land or conversion of forest land to non-forest use; or

- E. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of farmland to non-agricultural use or conversion of forest land to non-forest use?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The subject property is zoned RC-40, Resource Conservation Zone District, which is intended to conserve and protect natural resources and habitats. In accordance with its conservation purposes, the RC-40 Zone District limits lot coverage by permitted buildings and other structures to no more than one percent of the total lot area for all lots ten acres or larger. In this case, the property is approximately 357.80 acres, which would limit development to approximately 3.57 acres of building area. In this case, the existing and proposed improvements would not exceed that lot coverage threshold. The project will entail expansion of the existing service center to add an adjacent equipment storage yard, which will operate separately from the service center. The expansion will involve the removal of an approximately 2.62-acre forested area, clearing and grading of the site and paving. However, the proposed expansion area is less than significant.

III. AIR QUALITY

Where available, the significance criteria established by the applicable air quality management district or air pollution control district may be relied upon to make the following determinations. Would the project:

- A. Conflict with or obstruct implementation of the applicable Air Quality Plan; or
- B. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project proposes to construct an approximately 2.62-acre storage yard adjacent to the existing Southern California Edison Service Center. According to the project description, the construction of the storage yard is anticipated to last three months, and will involve grading the site, installation of concrete and gravel surfacing, the installation of two 1,440 square-foot mobile office/utility trailers, electrical equipment, water supply, septic system, an eight-foot-tall gated chain-link fence topped with barbed wire, and post-construction site clean-up.

It is expected that construction operation will involve emissions of criteria pollutants from vehicles, diesel-powered trucks and other construction equipment. However, due to the limited size of the proposed construction site and the short duration of the proposed construction timeline, project-related emissions are not anticipated to exceed any Air District established significance thresholds for criteria pollutants.

- C. Expose sensitive receptors to substantial pollutant concentrations; or

- D. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

FINDING: LESS THAN SIGNIFICANT IMPACT:

There is a residential development located approximately 200 feet west of the proposed storage yard; thus, it is possible that fugitive dust, particulate matter, and other emissions, which could result in odors, could impact sensitive receptors; however, based on the limited scope and time frame of construction activities, and the distance of the project site from residential dwellings, such impacts to sensitive receptors would be less than significant.

IV. BIOLOGICAL RESOURCES

Would the project:

- A. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service; or
- B. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

FINDING: LESS THAN SIGNIFICANT IMPACT:

This project was routed to the U.S. Fish and Wildlife Service and the California Department of Fish and Wildlife; neither agency expressed concerns that the project would have adverse impacts on any special-status species. No riparian habitat was identified by either reviewing agency.

According to conclusions of the applicant's Biological Habitat Assessment Report, which included a search of the California Department of Fish and Wildlife (CDFW), California Natural Diversity Database (CNDDDB); no current populations of the State Threatened Sierra Red Fox are known to occur in the vicinity of the project.

Additionally, habitat and elevation range preclude the State Threatened Tree Anemone (an evergreen shrub) from occurring in the proposed project area.

The State and Federal Candidate, Pacific Fisher (small carnivorous mammal), is known to occur in the project area; however, the applicant's Habitat Assessment Report indicates that no Pacific Fisher dens are known to occur on or adjacent to the project site, and the high level of anthropogenic activity in the project vicinity creates less than ideal habitat conditions for the Fisher.

State Listed/Threatened and Federally Endangered Sierra Nevada Yellow-Legged Frog is known to occur approximately one and one half-miles east of the project site.

The Bald Eagle is listed as California Endangered, and is known to occur in nesting pairs in the vicinity of Shaver Lake, approximately two miles northeast of the project site. The project site is not considered suitable nesting, roosting or foraging habitat for the Bald Eagle, and the project is not anticipated to result in adverse impacts to this species.

The California Endangered Great Grey Owl is known to occur within one to one and one half-miles of the project site; however, no suitable nesting habitat was observed within the project area.

The State Endangered Willow Flycatcher (migrant bird) is known to occur seasonally (in warmer months) in areas of similar elevation and vegetative cover as are found in the project vicinity; however, no habitat was observed within or adjacent to the project area. The Willow Flycatcher is protected by the Migratory Bird Treaty Act (MBTA).

The State Candidate (threatened) Foothill Yellow-Legged Frog is known to occur in areas similar in characteristics to the project area, typically found near a water source. No habitat was observed within the project area.

The California Spotted Owl (State Species of Special Concern) is known to occur in observed nesting pairs within three quarter-miles west of the project site.

The Northwestern Pond Turtle (State Species of Special Concern) is known to occur in areas with topographic and vegetative characteristics similar to the project site, although the site is at the species' upper elevation limit. The species is typically found near water sources; however, no habitat was observed within the project area.

- C. Have a substantial adverse effect on state or federally-protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

FINDING: NO IMPACT:

No state- or federally-protected wetlands were identified in the analysis.

- D. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

FINDING: LESS THAN SIGNIFICANT IMPACT:

None of the reviewing agencies with jurisdictional authority over the protection of biological resources expressed any concerns that this project would result in interference with migratory fish or wildlife species.

- E. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance; or

- F. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state Habitat Conservation Plan?

FINDING: NO IMPACT:

This proposal will not conflict with any local policies or ordinances protecting biological resources, nor conflict with any provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional or state Habitat Conservation Plan.

V. CULTURAL RESOURCES

Would the project:

- A. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5; or
- B. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5; or
- C. Disturb any human remains, including those interred outside of formal cemeteries?

FINDING: LESS THAN SIGNIFICANT IMPACT WITH MITIGATION INCORPORATED:

The project is in an area of moderate archaeological sensitivity; the project was routed to the Southern San Joaquin Information Center (SSJVIC) for review and comment. The SSJVIC determined the subject parcel was last surveyed in 2011, and recommended that a qualified professional perform a new archeological survey prior to project approval. Based on this recommendation, the applicant was asked to provide an Archaeological Survey for this project. The applicant's consultant evaluated the project site, performed a Cultural Historical Records Search and a pedestrian survey to determine the presence of any known cultural resources or previous cultural resource surveys on or near the subject parcel. The applicant's consultant, Material Culture Consulting, provided an archaeological survey dated June 10, 2019. Based on the results of the archeological survey, there were fifteen (15) previous cultural resource surveys within one quarter-mile of the project area and four (4) which encompassed portions of the project area; two (2) previously-recorded cultural resources were identified within a one quarter-mile radius of the project area. One of the identified resources is historical and the other is prehistoric. To address the possibility that previously-unknown subsurface cultural materials may be discovered during ground-disturbing activities, the following Mitigation Measure has been included, which will reduce potential impacts to cultural or historical resources to a less than significant level.

* **Mitigation Measure**

1. *In the event that cultural resources are unearthed during ground-disturbing activities, all work shall be halted in the area of the find. An archeologist shall be called to evaluate the findings and make any necessary mitigation recommendations. If human remains are unearthed during ground-disturbing activities, no further disturbance is to occur until the Fresno County Sheriff-Coroner has made the necessary findings as to origin and disposition. All normal evidence procedures should be followed by photos, reports, video, etc. If such remains are determined to be Native American, the Sheriff-Coroner must notify the Native American Commission within 24 hours.*

VI. ENERGY

Would the project:

- A. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources during project construction or operation; or
- B. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project proposes to remove a number of trees, grade a 2.62-acre area, add gravel surfacing and install two office trailers on concrete pads. It is expected that during the approximately three-month construction time frame, energy resources, electricity for lighting and fuel for vehicles and construction equipment will be utilized; however, it is not expected to be wasteful or unnecessary with adherence to standard construction practices. The project will not conflict with or obstruct a state or local plan for renewable energy.

VII. GEOLOGY AND SOILS

Would the project:

- A. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:
 1. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault; or
 2. Strong seismic ground shaking; or
 3. Seismic-related ground failure, including liquefaction; or
 4. Landslides?

FINDING: NO IMPACT:

The subject parcel is not located in an area designated as being prone to seismic activity in the Fresno County General Plan Background Report (FCGPBR).

B. Result in substantial soil erosion or loss of topsoil?

FINDING: LESS THAN SIGNIFICANT IMPACT:

Development of the site would involve grading which could result in some erosion, given the natural topography of the parcel; however, any such development will be required to obtain grading permits, and be subject to the requirements of the Grading and Drainage Sections of the Fresno County Ordinance Code.

- C. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse; or
- D. Be located on expansive soil as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?

FINDING: NO IMPACT:

The project site is not located within an area of known risk of landslide, lateral spreading, subsidence, liquefaction, collapse, or within an area of expansive soils.

- E. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project proposes to connect to an existing onsite septic system. This project was reviewed by the Fresno County Department of Public Health, Environmental Health Division, which did not express any concerns with the existing septic system.

- F. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

FINDING: NO IMPACT:

No paleontological resources or unique geologic features were identified in the analysis.

VIII. GREENHOUSE GAS EMISSIONS

Would the project:

- A. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment; or
- B. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project will generate greenhouse gas emissions during construction, and during operation; however, once construction is complete, additional vehicle traffic associated with the proposed storage yard is not anticipated to result in a substantial increase in long-term greenhouse gas emissions. The San Joaquin Valley Air Pollution Control District (SJVAPCD) published Guidance for Valley Land Use Agencies in Addressing GHG Emission Impacts for New Projects Under CEQA December 17, 2009.

The Guidance proposes the use of performance-based standards or Best Performance Standards (BPS) as a means of determining the significance of project specific GHG emission impacts by utilizing established design specification or project design elements, which would assist in identifying feasible GHG emission reduction or minimization measures. Emission reduction via implementation of BPS would be pre-quantified, eliminating the need for project-specific quantification of GHG emissions. Under these standards, this project will have a less than significant impact on Greenhouse Gas generation.

IX. HAZARDS AND HAZARDOUS MATERIALS

Would the project:

- A. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials; or
- B. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The proposed facility will be required to handle all hazardous waste in accordance with the provisions of California Code of Regulations (CCR), Title 22, Division 4.5.

- C. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one quarter-mile of an existing or proposed school?

FINDING: NO IMPACT:

The subject parcel is not located within one quarter-mile of an existing or proposed school.

- D. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

FINDING: NO IMPACT:

The subject property is not located on a hazardous materials site as identified by the U.S. Environmental Protection Agency, NEPAAssist mapping tool. The SCE Shaver Lake Service center is classified as a Hazardous Waste Generator under the guidelines of the Resource Conservation and Recovery Act (RCRA). The proposed material storage yard will contain electrical infrastructure materials such as power poles, wire and cable reels, insulators, new transformers, material crates, hardware, other palletized material, roll-off bins and two Conex storage boxes.

If any storage of hazardous materials is proposed, the applicant will be required to update its Hazardous Materials Business Plan within 30 days if there is a 100 percent increase in quantities of a previously-disclosed material, or the facility begins handling a previously-undisclosed material at or above the HMBP thresholds. All hazardous waste is required to be handled in accordance the provisions of the California Code of Regulations (CCR), Title 22, Division 4.5.

- E. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, result in a safety hazard or excessive noise for people residing or working in the project area?

FINDING: NO IMPACT:

The subject property is not located within an airport land use plan or within two miles of a public airport.

- F. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

FINDING: NO IMPACT:

The project will not impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan.

- G. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The proposed facility is in a State Responsibility Area (SRA) and is subject to all applicable SRA Fire Safe Regulations, which address such things as setbacks for structures, emergency access and vegetation management. The project was reviewed by CalFire, which did not express concerns that the project would result in an increased risk of human or structural exposure to wildfire that may result in loss, injury or death.

Because the subject parcel is located in area that is prone to wildfire risk, the potential remains for such an occurrence; however, the project does not entail a substantial increase in new structures or the addition of a substantial number of additional personnel over that of the existing facility, and with adherence to all applicable fire safe regulations and building and fire codes, such risks would be less than significant.

X. HYDROLOGY AND WATER QUALITY

Would the project:

- A. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?

FINDING: NO IMPACT:

The proposed facility is not anticipated to violate any water quality standards or waste discharge requirements, impacting surface or groundwater. The project site is located approximately one third-mile southwest of the nearest extent of Shaver Lake.

- B. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

FINDING: NO IMPACT:

The proposed facility will provide storage for electrical infrastructure materials and equipment, and does not propose to use substantial quantities of groundwater in its operation.

- C. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

1. Result in substantial erosion or siltation on or off site; or
2. Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on or off site; or
3. Create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff; or
4. Impede or redirect flood flows?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project will involve the removal of trees and vegetation, grading, leveling and surfacing of the approximately 2.62-acre project site, which will alter the drainage pattern of the site from its previously natural state. However, the majority of the site will be surfaced with gravel over the native soil, with the exception of concrete pads for parking spaces, thus maintaining a predominately pervious surface, minimizing the amount of additional runoff created by development of the site. The project will not alter the course of a stream or river, nor is it anticipated to result in substantial offsite erosion or siltation. Any additional runoff is not anticipated to exceed the capacity of existing or planned storm water drainage systems and must be retained on site in compliance with County standards.

- D. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

FINDING: NO IMPACT:

The project site is not located in a flood hazard area or an area at risk of tsunami or seiche.

- E. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

FINDING: NO IMPACT:

The project is not anticipated to increase the use of groundwater in excess of the existing facility's current water use.

XI. LAND USE AND PLANNING

Would the project:

- A. Physically divide an established community; or
- B. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

FINDING: NO IMPACT:

The project will not physically divide an established community or conflict with any land use plan, policy or regulation adopted for avoiding or mitigating environmental impacts.

XII. MINERAL RESOURCES

Would the project:

- A. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state; or

- B. Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local General Plan, Specific Plan or other land use plan?

FINDING: NO IMPACT:

The subject parcel is not located in an area of known mineral resources as identified by Figures 7-7 and 7-8 of the Fresno County General Plan Background Report (FCGPBR).

XIII. NOISE

Would the project result in:

- A. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies; or
- B. Generation of excessive ground-borne vibration or ground-borne noise levels?

FINDING: LESS THAN SIGNIFICANT IMPACT:

Project construction activities may generate temporary ambient noise levels including ground-borne vibration; however, as construction activity will be limited to a 2.62-acre area adjacent to the existing SCE Service Center, and with a limited construction timeline, no substantial temporary or permanent increase in noise is anticipated. The project will be required to comply with the Fresno County Ordinance Code.

- C. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

FINDING: NO IMPACT:

The project site is not located in the vicinity of a private airstrip or within the review area of an airport land use plan.

XIV. POPULATION AND HOUSING

Would the project:

- A. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure); or
- B. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

FINDING: NO IMPACT:

The project proposal, once constructed, will employ approximately 25 people in the operation of the storage yard. The proposal is not anticipated to induce substantial population growth, and no new infrastructure other than an access driveway for the storage yard is proposed. No housing or people will be displaced as a result of this project.

XV. PUBLIC SERVICES

Would the project:

- A. Result in substantial adverse physical impacts associated with the provision of new or physically-altered governmental facilities, or the need for new or physically-altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the following public services:
 - 1. Fire protection;
 - 2. Police protection;
 - 3. Schools;
 - 4. Parks; or
 - 5. Other public facilities?

FINDING: NO IMPACT:

The project will not result in adverse impacts associated with the provision of any government facilities, or result in the need for new governmental facilities, the construction of which would cause adverse impacts to the provision of public services. No reviewing agencies expressed concerns that the project would adversely impact public services.

XVI. RECREATION

Would the project:

- A. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated; or
- B. Include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment?

FINDING: NO IMPACT:

The project proposes the expansion of an existing Southern California Service Center, with the addition of an approximately 2.62-acre outdoor storage yard to be constructed adjacent the Service Center. This proposal is not anticipated to increase the use of existing parks or recreational facilities, nor will it involve the construction or expansion of such facilities.

XVII. TRANSPORTATION

Would the project:

- A. Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities; or
- B. Be in conflict or be inconsistent with the California Environmental Quality Act (CEQA) Guidelines Section 15064.3, subdivision (b); or
- C. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment); or
- D. Result in inadequate emergency access?

FINDING: NO IMPACT:

The proposed project will take access via a new driveway, connecting to a private road which provides access from Dinkey Creek Road. The proposed access drive will be subject to all applicable County development standards and State Responsibility Area Fire Safe Regulations as they pertain to emergency access standards. The project was not evaluated using the Vehicle Miles Travelled metric, and no traffic study was required. The project proposes a 2.62-acre addition to an existing facility and will not conflict with any program, plan, ordinance or policy which addresses the County circulation system. No work within the County right-of-way is proposed. No changes with the configuration of the existing private access road from Dinkey Creek Road is proposed with this application.

XVIII. TRIBAL CULTURAL RESOURCES

Would the project:

- A. Cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code Section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

1. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k); or
2. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1? (In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.)

FINDING: LESS THAN SIGNIFICANT IMPACT WITH MITIGATION INCORPORATED:

Under the provisions of Assembly Bill 52, the County of Fresno was required to provide notice that this Initial Study was being prepared to Native American Tribes who had previously indicated interest in reviewing CEQA projects. Notices were sent on April 30, 2019, to Robert Ledger of the Dumna Wo Wah, Robert Pennell of Table Mountain Rancheria, Ruben Barrios of Santa Rosa Rancheria and to Tara Estes-Harter of the Picayune Rancheria of Chukchansi Indians. None of the Tribal Governments responded to the notice.

The project site is in an area of moderate archeological sensitivity; therefore, the following Mitigation Measure is proposed to ensure that potential impacts to previously-unknown tribal cultural resources can be reduced to less than significant.

* **Mitigation Measure**

1. See *Mitigation Measure No. 1, Section V above.*

XIX. UTILITIES AND SERVICE SYSTEMS

Would the project:

- A. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

FINDING: NO IMPACT:

The proposed project will not require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, natural gas, or telecommunications facilities. The proposed laydown storage yard will be constructed and operated by Southern California Edison, a public utility. There is new electrical service proposed to supply the storage yard lighting and portable office/utility trailers with electrical power. Additionally, the project proposes to connect to an existing onsite septic system to serve the portable office/utility trailers.

- B. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?

FINDING: NO IMPACT:

The project proposes the expansion of an existing Service Center facility with the addition of a paved storage yard with minor improvements for the storage of materials and equipment for the repair and maintenance of its electrical infrastructure. The proposed expansion will involve the addition of two portable utility trailers, which will entail minimal additional water use. The applicant's operational statement anticipates 60 gallons per day. Water is supplied to the existing facility by the Shaver Lake Heights Mutual Water Company. No concerns related to water supply for this project proposal were expressed by any reviewing agencies or departments.

- C. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

FINDING: NO IMPACT:

The project proposes to utilize an existing onsite septic system. No expanded capacity is anticipated with this proposal. The existing Service Center is provided sewer services through County Service Area (CSA) 31B.

- D. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals; or
- E. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

FINDING: NO IMPACT:

The project is not anticipated to generate solid waste in excess of State or local standards, or in excess of local infrastructure capacity; additionally, the project will be subject to the provisions of the Fresno County Ordinance Code, Title 8.20, pertaining to Solid Waste Disposal.

XX. WILDFIRE

If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

- A. Substantially impair an adopted emergency response plan or emergency evacuation plan, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

FINDING: NO IMPACT:

The project will not impair an adopted emergency response or emergency evacuation plan, or impair any existing or planned telecommunication facilities. The project involves the expansion of an existing facility which will take access from a private road which serves the existing facility. The proposal will be subject to all applicable SRA Fire Safe Regulations, Title 15.60 Fresno County Ordinance Code, including design of emergency access, and California Code of Regulations (CCR) Title 24-Fire Code.

- B. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The subject parcel is in a forested area where there is substantial risk of wildfire occurrence. The project site is situated in an area of gently to moderately sloping terrain, and adjacent to the intersection of two roads, Dinkey Creek Road and the private road serving the existing facility. The project was reviewed by CalFire, which did not express any specific concerns related to increased wildfire risks due to slope or prevailing winds.

The project proposes the expansion of an existing operation, with the addition of storage area for equipment and materials used for the maintenance of its local electrical infrastructure. It is unknown whether the storage of such materials and equipment creates and increased risk of wildfire, or in such a case result in the exposure of people working or living near the site to pollutant concentrations. The project proponent will be required to submit plans for review and approval by CalFire prior to the issuance of building permits.

- C. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project will be required to comply with all applicable State Responsibility Area (SRA) fire safe regulations, including, but not limited to, setbacks for structures, road improvements, emergency access, flammable vegetation management, and water supply.

- D. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

FINDING: LESS THAN SIGNIFICANT IMPACT:

The project site is in an area where slopes may exceed thirty percent, according to Figure 7-2 of the Fresno County General Plan Background Report; however, the project site and immediate vicinity appear from site photos in an aerial imagery to be gently to moderately sloping. The project site is not in an area at risk of flood inundation due to dam failure, and according to FEMA, FIRM Panel 0725H the project site is in an area of minimal flood hazard. The project will be required to obtain grading permits for any grading proposed with the project, and may require an engineered grading and drainage plan. Additionally, the proposed 2.62-acre storage yard will be surfaced with compacted gravel, and parking areas will be paved with concrete.

XXI. MANDATORY FINDINGS OF SIGNIFICANCE

Would the project:

- A. Have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

FINDING: LESS THAN SIGNIFICANT IMPACT WITH MITIGATION INCORPORATED:

The project entails the expansion of an existing Southern California Edison Service Center, which entails the construction of an approximately 2.62-acre equipment and materials storage yard, adjacent to the service center. The proposal will add some outdoor security lighting, and as such, Mitigation has been included requiring all outdoor lighting to be hooded and directed so as not to affect adjacent property or the roadway. To address the possibility that previously-undiscovered subsurface paleontological, cultural/historical or tribal/cultural resources are present within the project area, additional Mitigation has been included under Section V, which implements avoidance and reporting measures, which will reduce potential impacts to a less than significant level.

* **Mitigation Measure(s)**

1. *See Section I.*
 2. *See Section V.*
 3. *See Section XVIII.*
- B. Have impacts that are individually limited, but cumulatively considerable (“cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?

FINDING: NO IMPACT:

No cumulatively considerable impacts were identified in the analysis that would result from the project.

- C. Have environmental effects which will cause substantial adverse effects on human beings either directly or indirectly?

FINDING: NO IMPACT:

No environmental effects that would result in substantial adverse impacts to human beings were identified in the analysis.

CONCLUSION/SUMMARY

Based upon the Initial Study prepared for Unclassified Conditional Use Permit Application No. 3639, staff has concluded that the project will not have a significant effect on the environment. It has been determined that there would be no impacts to Land Use and Planning, Mineral Resources, Population and Housing, Public Services, Recreation, and Utilities and Service Systems.

Potential impacts related to Agriculture and Forestry Resources, Air Quality, Biological Resources, Energy, Hazards and Hazardous Materials, Geology and Soils, Hydrology and Water Quality, Noise, Greenhouse Gas Emissions, Transportation, and Wildfire have been determined to be less than significant.

Potential impacts relating to Aesthetics, Cultural Resources, and Tribal Cultural Resources have been determined to be less than significant with compliance with noted Mitigation Measures.

A Mitigated Negative Declaration is recommended and is subject to approval by the decision-making body. The Initial Study is available for review at 2220 Tulare Street, Suite A, street level, located on the southwest corner of Tulare and "M" Street, Fresno, California.

JS:ksn

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File original and one copy with: Fresno County Clerk 2221 Kern Street Fresno, California 93721		Space Below For County Clerk Only. CLK-2046.00 E04-73 R00-00	
Agency File No: IS 7593	LOCAL AGENCY MITIGATED NEGATIVE DECLARATION		County Clerk File No: E-
Responsible Agency (Name): Fresno County	Address (Street and P.O. Box): 2220 Tulare St. Sixth Floor	City: Fresno	Zip Code: 93721
Agency Contact Person (Name and Title): Jeremy Shaw, Planner	Area Code: 559	Telephone Number: 600-4207	Extension: N/A
Project Applicant/Sponsor (Name): Pascual Garcia/ Southern California Edison	Project Title: Initial Study Application No. 7593/Unclassified Conditional Use Permit No. 3639		
Project Description: Allow the expansion of an existing Southern California Edison, Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution system on a 2.62-acre portion of a 357.80-acre parcel in the RC-40 (Resource Conservation, 40-acre minimum parcel size) Zone District. The subject parcel is located on the north side of Dinkey Creek Road, approximately one quarter-mile east of State Route 168/Tollhouse Road, within the unincorporated community of Shaver Lake (SUP. DIST. 5) (APN 120-260-10U) (41694 Dinkey Creek Road).			
Justification for Negative Declaration: Based upon the Initial Study prepared for Unclassified Conditional Use Permit Application No. 3635, staff has concluded that the project will not have a significant effect on the environment. It has been determined that there would be no impacts to Land Use and Planning, Mineral Resources, Population and Housing, Public Services, Recreation, Utilities and Service Systems. Potential impacts related to Agriculture and Forestry Resources, Air Quality, Biological Resources, Energy, Hazards and Hazardous Materials, Geology and Soils, Hydrology and Water Quality, Noise, Greenhouse Gas Emissions, Transportation and Wildfire have been determined to be less than significant. Potential impacts relating to, Aesthetics, Cultural Resources and Tribal Cultural Resources have determined to be less than significant with compliance with noted Mitigation Measures. A Mitigated Negative Declaration is recommended and is subject to approval by the decision-making body. The Initial Study is available for review at 2220 Tulare Street, Suite A, street level, located on the southwest corner of Tulare and "M" Street, Fresno, California.			
FINDING: The proposed project will not have a significant impact on the environment.			
Newspaper and Date of Publication: Fresno Business Journal – November 20, 2019		Review Date Deadline: Planning Commission – January 9, 2020	
Date:	Type or Print Signature: Marianne Mollring Senior Planner	Submitted by (Signature): Planner	

State 15083, 15085

County Clerk File No.: _____

**LOCAL AGENCY
MITIGATED NEGATIVE DECLARATION**



County of Fresno

DEPARTMENT OF PUBLIC WORKS AND PLANNING
STEVEN E. WHITE, DIRECTOR

DATE: September 26, 2019

TO: Development Services and Capital Projects, Attn: William M. Kettler, Division Manager
Development Services and Capital Projects, Attn: Chris Motta, Principal Planner
Development Services and Capital Projects, Current Planning, Attn: Marianne Mollring, Senior Planner
Development Services and Capital Projects, Policy Planning, ALCC, Attn: Mohammad Khorsand
Development Services and Capital Projects, Zoning & Permit Review, Attn: Daniel Gutierrez
Development Services and Capital Projects, Site Plan Review, Attn: Hector Luna
Development Services and Capital Projects, Building & Safety/Plan Check, Attn: Chuck Jonas
Development Engineering, Attn: Laurie Kennedy, Grading/Mapping
Road Maintenance and Operations, Attn: John Thompson/Wendy Nakagawa/Nadia Lopez
Design Division, Transportation Planning, Attn: Mohammad Alimi/Dale Siemer/Brian Spaunhurst
Resources Division/Special Districts, Attn: Amina Flores-Becker, Daniel Vang
Water and Natural Resources Division, Attn: Glenn Allen, Division Manager
Department of Public Health, Environmental Health Division, Attn: Kevin Tsuda/Deep Sidhu/Steven Rhodes
Agricultural Commissioner, Attn: Melissa Cregan
U.S. Fish and Wildlife Service, San Joaquin Valley Division, Attn: Patricia Cole, Matthew Nelson
CA Regional Water Quality Control Board, Attn: Dale Harvey
Fresno County Sheriffs Department, Attn: John Zanoni, John Reynolds, Louis Hernandez, Kathy Curtis, Ryan Hushaw
CA Department of Fish and Wildlife, Attn: Renee Robison, Environmental Scientist
California Department of Transportation, District 6, Attn: David Padilla
Dumna Wo Wah Tribal Government, Attn: Robert Ledger, Tribal Chairperson
Picayune Rancheria of the Chukchansi Indians, Attn: Heather Airey, THPO/Cultural Resources Director
Table Mountain Rancheria, Attn: Leanne Walker-Grant, Tribal Chairperson
Robert Pennell, Cultural Resources Director/Kim Taylor, Cultural Resources Department/Sara Barnett, Cultural Resources Department
Department
Santa Rosa Rancheria Tachi-Yokut Tribe, Attn: Hector Franco, Director/Shana Powers, Cultural Specialist
San Joaquin Valley Unified Air Pollution Control District (PIC-CEQA Division), Attn: PIC Supervisor
Sierra Resource Conservation District, Attn: Steve Haze
Fresno County Fire Protection District, Attn: Jim McDougald, Division Chief
Native American Heritage Commission (NAHC), Attn: Katy Sanchez

U.S. Army Corps of Engineers, Pine Flat Lake, Attn: Robert Lafrentz, Senior Park Ranger
U.S. Army Corps of Engineers, Sacramento District, Regulatory Division, CA South Branch, Attn: Kathleen A Dadey, Chief
U.S. Forest Service, Sierra National Forest, Attn: Dean A. Gould, Antonio Cabrera

FROM: Jeremy Shaw, Planner JS
Development Services and Capital Projects Division

SUBJECT: Initial Study Application No. 7593, Unclassified Conditional Use Permit Application No. 3639 (Amending CUP 3487)

APPLICANT: Pascual Garcia/Southern California Edison

DUE DATE: October 10, 2019

The Department of Public Works and Planning, Development Services and Capital Projects Division is reviewing the subject applications proposing to allow the expansion of an existing Southern California Edison, Service Center with the construction of a laydown yard, which will provide storage area for equipment and material for the construction and maintenance of Southern California Edison's Transmission and Distribution organization. The propose laydown yard will operate separately from the Service Center, on a 2.62-acre portion of a 357.80-acre parcel, in the RC-40 (Recreational, 40-acre minimum parcel size) Zone District (APN's 120-260-10U) (SUP. DIST. 5) (41694 Dinkey Creek Road, Shaver Lake).

Note: this project was previously routed for comment on April 18, 2019. Since that time the proposed project has been revised to include the addition of two new 24-foot wide by 60-foot long by 14-foot tall, mobile office trailers. Additionally, an Initial Study will be completed in accordance with the provision of the California Environmental Quality Act (CEQA).

This application, if approved would amend previously approved Conditional Use Permit No. 3487.

The Department of Public Works and Planning, Development Services Division is also reviewing this proposal for environmental effects as mandated by the California Environmental Quality Act (CEQA) and for conformity with the plans and policies of the County of Fresno.

Based upon this review, a determination will be made regarding conditions to be imposed on the project, including necessary on-site and off-site improvements.

We must have your comments by **October 10, 2019**. Any comments received after this date may not be used.

NOTE - THIS WILL BE OUR ONLY REQUEST FOR WRITTEN COMMENTS. If you do not have comments, please provide a "NO COMMENT" response to our office by the above deadline (e-mail is also acceptable; see email address below).

Please address any correspondence or questions related to environmental and/or policy/design issues to me, Jeremy Shaw, Planner, Development Services and Capital Projects Division, Fresno County Department of Public Works and Planning, 2220 Tulare Street, Sixth Floor, Fresno, CA 93721, or call (559) 600-4207, or email jshaw@FresnoCountyCA.gov.

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Activity Code (Internal Review):2384



Fresno County Department of Public Works and Planning

Date Received: 9-26-2019

CUP 3639

(Application No.)

MAILING ADDRESS:

Department of Public Works and Planning
Development Services and Capital Projects Division
2220 Tulare St., 6th Floor
Fresno, Ca. 93721

LOCATION:

Southwest corner of Tulare & "M" Streets, Suite A
Street Level
Fresno Phone: (559) 600-4497

APPLICATION FOR:

- Pre-Application (Type)
Amendment Application
Amendment to Text
Conditional Use Permit
Variance (Class)/Minor Variance
Site Plan Review/Occupancy Permit
No Shoot/Dog Leash Law Boundary
General Plan Amendment/Specific Plan/SP Amendment
Time Extension for
Director Review and Approval
for 2nd Residence
Determination of Merger
Agreements
ALCC/RLCC
Other

DESCRIPTION OF PROPOSED USE OR REQUEST:

Construction of a new material laydown yard for Southern California Edison located at the existing Shaver Lake Service Center in Shaver Lake, California.

CEQA DOCUMENTATION: Initial Study PER N/A

PLEASE USE FILL-IN FORM OR PRINT IN BLACK INK. Answer all questions completely. Attach required site plans, forms, statements, and deeds as specified on the Pre-Application Review. Attach Copy of Deed, including Legal Description.

LOCATION OF PROPERTY: Northeast side of Dinkey Creek Road
between SCE Shaver Lake Service Center and Cal Fire Shaver Lake Station
Street address: 41694 Dinkey Creek Road Shaver Lake, CA 93664

APN: 120-260-10U Parcel size: 357.80 +/- Acres Section(s)-Twp/Rg: S 36 - T 9 S/R 24 E

ADDITIONAL APN(s):

I, Pascual Garcia (signature), declare that I am the owner, or authorized representative of the owner, of the above described property and that the application and attached documents are in all respects true and correct to the best of my knowledge. The foregoing declaration is made under penalty of perjury.

Table with 5 columns: Role, Name, Address, City, Zip, Phone. Rows for Owner (Pascual Garcia), Applicant (Pascual Garcia), and Representative (Pascual Garcia).

CONTACT EMAIL: pascual.garcia@sce.com

OFFICE USE ONLY (PRINT FORM ON GREEN PAPER)

Application Type / No.: UCLUP No. 3639 (REV) Fee: \$1,140.25
Application Type / No.: Fee: \$
Application Type / No.: Fee: \$
Application Type / No.: Fee: \$
PER/Initial Study No.: IS. No 7593 Fee: \$5,151.00
Ag Department Review: Fee: \$
Health Department Review: Fee: \$338.00
Received By: JS Invoice No.: 123253 TOTAL: \$6,629.25

UTILITIES AVAILABLE:

WATER: Yes [X] / No []
Agency:
SEWER: Yes [X] / No []
Agency: CSA 31B Shaver Lake Sewer

STAFF DETERMINATION: This permit is sought under Ordinance Section:

Sect-Twp/Rg: - T S/R E

Related Application(s):

APN # - -

Zone District: RC-40

APN # - -

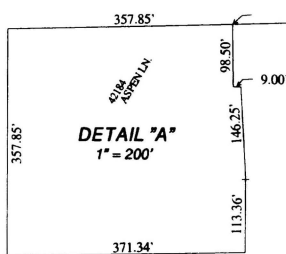
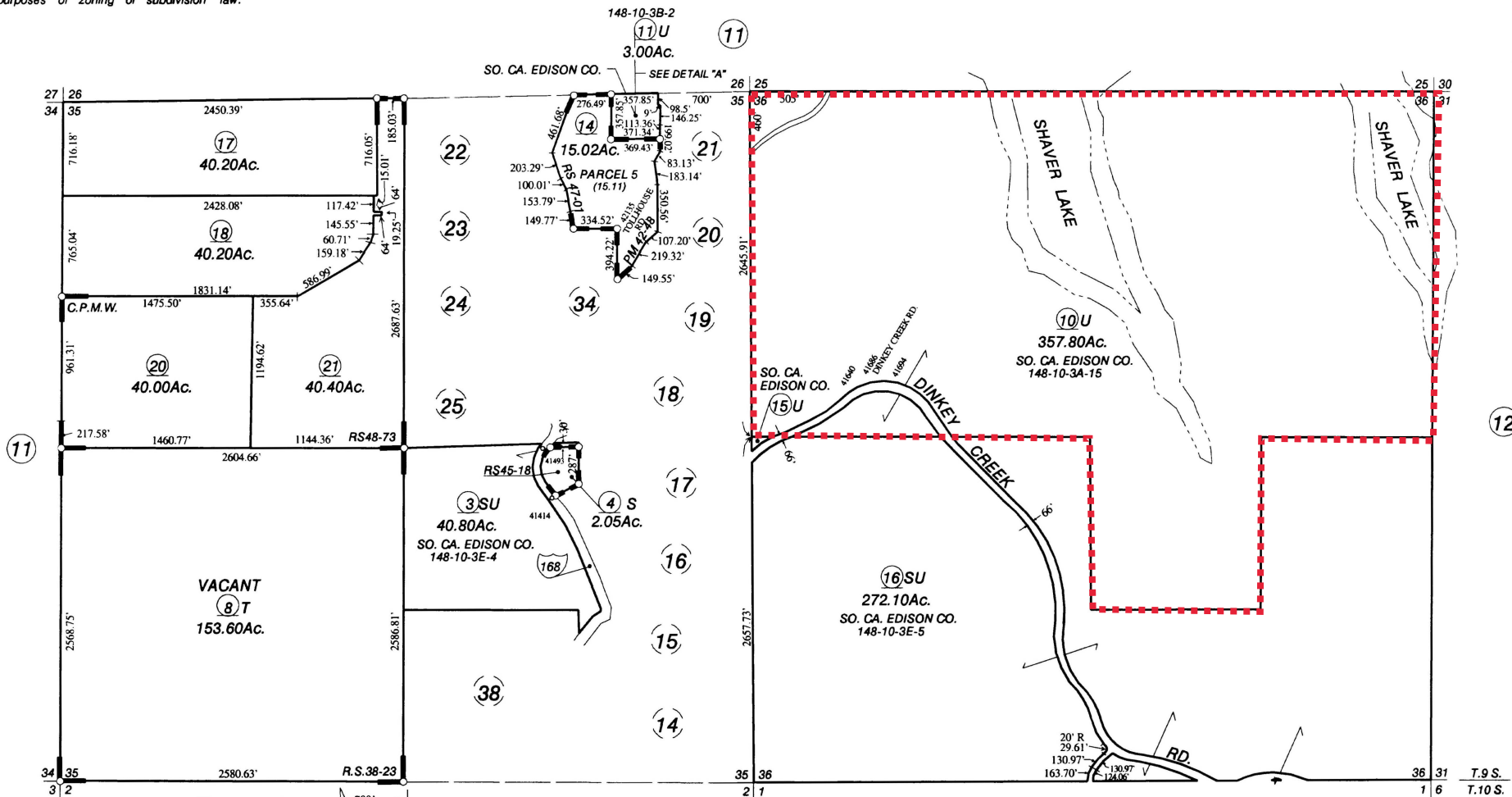
Parcel Size:

APN # - -

APN # - -

over.....

--- NOTE ---
This map is for Assessment purposes only.
It is not to be construed as portraying
legal ownership or divisions of land for
purposes of zoning or subdivision law.



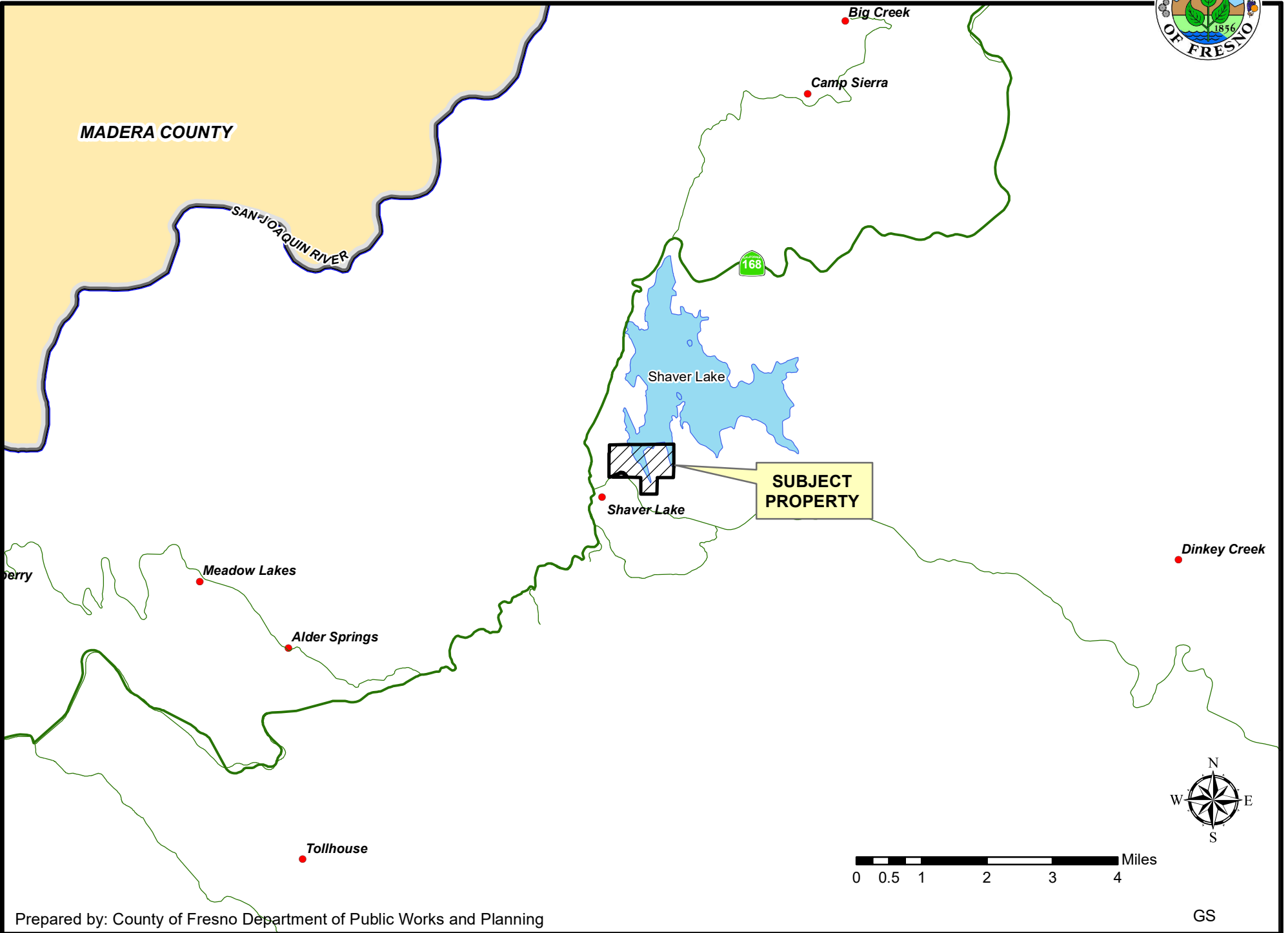
Bk.
130

Certificate of Parcel Map Waiver No. 02-04, Doc. No. 50701, 3-7-05
Parcel Map No.6118 - Bk.42, Pg.48
Record of Survey - Bk.38, Pgs.23-31
Record of Survey - Bk.45, Pg.18
Record of Survey - Bk.47, Pg.01
Record of Survey - Bk. 48, Pg. 73

Assessor's Map Bk.120 - Pg. 26
County of Fresno, Calif.

NOTE - Assessor's Block Numbers Shown in Ellipses.
Assessor's Parcel Numbers Shown in Circles.

LOCATION MAP





County of

DEPARTMENT OF PUBLIC WORKS AND PLANNING
Fresno, DIRECTOR

INITIAL STUDY APPLICATION

INSTRUCTIONS

Answer all questions completely. An incomplete form may delay processing of your application. Use additional paper if necessary and attach any supplemental information to this form. Attach an operational statement if appropriate. This application will be distributed to several agencies and persons to determine the potential environmental effects of your proposal. Please complete the form in a legible and reproducible manner (i.e., USE BLACK INK OR TYPE).

OFFICE USE ONLY

IS No. _____

Project No(s). _____

Application Rec'd.: _____

GENERAL INFORMATION

1. **Property Owner :** Southern California Edison **Phone/Fax** _____

Mailing

Address: 8631 Rush Street Rosemead CA 91770
Street City State/Zip

2. **Applicant :** Pascual Garcia **Phone/Fax:** 909-440-0880

Mailing

Address: 800 West Cienega Ave. San Dimas CA 91773
Street City State/Zip

3. **Representative:** Pascual Garcia **Phone/Fax:** 909-440-0880

Mailing

Address: 800 West Cienega Ave. San Dimas CA 91773
Street City State/Zip

4. **Proposed Project:** Construction of a new material laydown yard for Southern California Edison located at the existing Shaver Lake

Service Center in Shaver Lake, California.

5. **Project Location:** Northeast side of Sparrow Road and Dinkey Creek Road between SCE's Shaver Lake Service Center and Cal Fire

Shaver Lake Station.

6. **Project Address:** 41694 Dinkey Creek Road Shaver Lake, CA 93664

7. **Section/Township/Range:** 36 / 9 / 24 8. **Parcel Size:** 357.80 +/- Acres

9. **Assessor's Parcel No.** 120-260-10U **OVER.....**

10. Land Conservation Contract No. (If applicable): NIA

11. What other agencies will you need to get permits or authorization from:

<input type="checkbox"/> LAFCo (annexation or extension of services)	<input type="checkbox"/> SJVUAPCD (Air Pollution Control District)
<input type="checkbox"/> CALTRANS	<input type="checkbox"/> Reclamation Board
<input type="checkbox"/> Division of Aeronautics	<input type="checkbox"/> Department of Energy
<input checked="" type="checkbox"/> Water Quality Control Board	<input type="checkbox"/> Airport Land Use Commission
<input type="checkbox"/> Other _____	

12. Will the project utilize Federal funds or require other Federal authorization subject to the provisions of the National Environmental Policy Act (NEPA) of 1969? Yes No

If so, please provide a copy of all related grant and/or funding documents, related information and environmental review requirements.

13. Existing Zone District¹: RC-40 (Resource Conservation District, 40-acre minimum parcel size)

14. Existing General Plan Land Use Designation¹: Public Land Use-existing SCE Service Center

ENVIRONMENTAL INFORMATION

15. Present land use: Currently open field
Describe existing physical improvements including buildings, water (wells) and sewage facilities, roads, and lighting. Include a site plan or map showing these improvements:

Proposed improvements: site grading, rock surface, concrete driveway, concrete pads, electrical power, communication, septic, water, two modular office trailer, stairs, ADA ramps and parking, deck/landing, connex storage boxes and trash/recycle bins.

Describe the major vegetative cover: _____

Any perennial or intermittent water courses? If so, show on map: _____

Is property in a flood-prone area? Describe:

16. Describe surrounding land uses (e.g., commercial, agricultural, residential, school, etc.):

North: Fresno County maintenance yard

South: Dinkey Creek Road and south of that is open land


East: Cal Fire Shaver Lake Station


West: SCE Shaver Lake Service Center

24. **Anticipated volume of water to be used (gallons per day)²:** During construction grading approximately 16,000 gallons per day
Once the yard is operational approximately 60 gallons per day.
During winter months approximately 24 gallons per day.
25. **Proposed method of liquid waste disposal:**
 septic system/individual
 community system³-name _____
26. **Estimated volume of liquid waste (gallons per day)²:** 60 gallons and during winter months approximately 24 gallons. Estimated to
be 3 Porta-Potty on site with approximated 60 gallons each and will be
service weekly for a total of 180 gallons per week.
27. **Anticipated type(s) of liquid waste:** Water and bodily fluids/waste _____
28. **Anticipated type(s) of hazardous wastes²:** No know hazardous waste at this time _____
29. **Anticipated volume of hazardous wastes²:** No know hazardous waste at this time _____
30. **Proposed method of hazardous waste disposal²:** N/A _____
31. **Anticipated type(s) of solid waste:** Deteriorated wood poles and related hardware, wire and cable and typical daily trash _____
32. **Anticipated amount of solid waste (tons or cubic yards per day):** Approximately 3 tons per day, during the winter months
approximately 1.2 tons
33. **Anticipated amount of waste that will be recycled (tons or cubic yards per day):** Approximately 100 pounds and 40
pounds during winter months
34. **Proposed method of solid waste disposal:** Qualified SCE vendor is Granite Solid Waste _____
35. **Fire protection district(s) serving this area:** Cal Fire and US Forest Service _____
36. **Has a previous application been processed on this site? If so, list title and date:** _____

37. **Do you have any underground storage tanks (except septic tanks)?** Yes _____ No _____
38. **If yes, are they currently in use?** Yes _____ No _____

TO THE BEST OF MY KNOWLEDGE, THE FOREGOING INFORMATION IS TRUE.


SIGNATURE


DATE

¹Refer to Development Services and Capital Projects Conference Checklist
²For assistance, contact Environmental Health System, (559) 600-3357
³For County Service Areas or Waterworks Districts, contact the Resources Division, (559) 600-4259

(Revised 12/14/18)

NOTICE AND ACKNOWLEDGMENT

INDEMNIFICATION AND DEFENSE

The Board of Supervisors has adopted a policy that applicants should be made aware that they may be responsible for participating in the defense of the County in the event a lawsuit is filed resulting from the County's action on your project. You may be required to enter into an agreement to indemnify and defend the County if it appears likely that litigation could result from the County's action. The agreement would require that you deposit an appropriate security upon notice that a lawsuit has been filed. In the event that you fail to comply with the provisions of the agreement, the County may rescind its approval of the project.

STATE FISH AND WILDLIFE FEE

State law requires that specified fees (effective January 1, 2019: \$3,271.00 for an EIR; \$2,354.75 for a Mitigated/Negative Declaration) be paid to the California Department of Fish and Wildlife (CDFW) for projects which must be reviewed for potential adverse effect on wildlife resources. The County is required to collect the fees on behalf of CDFW. A \$50.00 handling fee will also be charged, as provided for in the legislation, to defray a portion of the County's costs for collecting the fees.

The following projects are exempt from the fees:

- 1. All projects statutorily exempt from the provisions of CEQA (California Environmental Quality Act).*
- 2. All projects categorically exempt by regulations of the Secretary of Resources (State of California) from the requirement to prepare environmental documents.*

A fee exemption may be issued by CDFW for eligible projects determined by that agency to have "no effect on wildlife." That determination must be provided in advance from CDFW to the County at the request of the applicant. You may wish to call the local office of CDFW at (559) 222-3761 if you need more information.

Upon completion of the Initial Study you will be notified of the applicable fee. Payment of the fee will be required before your project will be forwarded to the project analyst for scheduling of any required hearings and final processing. The fee will be refunded if the project should be denied by the County.



Applicant's Signature

8.14.19

Date



Shaver Lake Laydown Yard
41694 Dinkey Creek Road
Shaver Lake, CA 93664
August 14, 2019

Site/Project Information
Unclassified Conditional Use Permit #3639 (Rev. 1)
APN #120-260-10U

Operational Statement for Proposed Laydown Yard

SCE needs to construct a laydown yard to support upcoming and ongoing project work in the Shaver Lake region.

The laydown yard will be adjacent to SCE's existing Service Center, but the Service Center's operations will remain unchanged and unaffected. The laydown yard will operate separately from the existing Service Center.

Currently the existing Service Center consists of one building that houses the floor area for office space, warehouse use, garage use and a small open material storage yard. The existing Service Center operations consist of administrative service activities which include customer service functions and the dispatching of service vehicles involved with the construction and day to day maintenance of SCE's electrical system.

- 1. Nature of the Operation:** Southern California Edison (SCE) would like to operate a laydown yard to support our Transmission Line Rating Remediation (TLRR) program and future corporate operational needs. SCE's operational statement will only address the area on the parcel that we are proposing to use as a laydown yard. The proposed Shaver Lake laydown yard consists of 2.62 acres of enclosed area for use by SCE's Transmission and Distribution organization. The proposed laydown yard will provide the necessary storage area for equipment and material for the construction and maintenance of SCE's electrical system. Construction material will be delivered to this site and redistributed to the ongoing active construction areas. Crews will enter the laydown yard to pick up material and exit the yard after loading material.
- 2. Operational Time Limits:** The proposed laydown yard will operation from 6 a.m. to 6 p.m. during normal operation. The workers will work 12 hours per day up to 6 days a week. However, workers may utilize the yard beyond normal hours if the work is urgent and/or time sensitive.
- 3. Number of Customer Visitors:** No customers will visit the laydown yard. The private road the driveway for the laydown yard will be accessed from also provides access to the

SCE Service Center and Fresno County yard. The SCE Service Center does have approximately 10-15 customer visitors a day during business hours 7am – 4pm, Monday – Friday. The EIR for the SCE Service Center addressed the customer traffic.

4. **Number of employees:** SCE estimates there will be 25 workers on site once the yard is operational. Crews will leave the yard with the line trucks and relate equipment in the morning and return in the afternoon during normal hours of operation. No workers will live on site.
5. **Service and Delivery Vehicles:** Delivery vehicles will deliver packages, pallets, hardware, poles, electrical apparatus, etc. Deliveries are expected 1-2 times per day, approximately 2-4 days per week.
6. **Access to the site:** The site address is located at 41694 Dinkey Creek Road approximately a quarter mile east of Highway 168. The proposed concrete private access road will be located off a paved private road, north of the Shaver Lake SCE Service Center employee entrance.
7. **Number of parking spaces:** The proposed laydown yard will have two ADA compliant parking stalls (one for each office trailer) and open parking spaces on asphalt, concrete and/or gravel for the remaining twenty three workers.
8. **Are any goods to be sold on site?** No.
9. **What equipment is used?** The proposed laydown yard will use one or two all-terrain forklift which will be stored within the laydown yard. Depending on work load, the equipment stored and driven off daily includes one crane truck, four bucket trucks, four F550 trucks and nine $\frac{3}{4}$ or $\frac{1}{2}$ ton trucks.
10. **What supplies or materials are used and how are they stored?** SCE will be storing power poles, wire and cable reels, insulators, new transformers, material crates, hardware, material on pallets, two roll-off bins and approximately two conex storage box.
11. **Does the use cause an unsightly appearance?** No. Due to heavy foliage around the site and its distance from the main road, it is not easily seen.
12. **List any solid or liquid waste to be produced:** Liquid waste produced by the laydown yard will come from the portable restroom uses. Solid waste includes wood, metal, cardboard, paper, etc. The produced waste disposal will comply with local county ordinances.
13. **Estimated volume of water to be used:** The proposed laydown yard restroom will use approximately 60 gallons of non-potable water per day.
14. **Describe any proposed advertising including size, appearance, and placement:** The site will have no advertising.
15. **Will existing buildings be used or will new buildings be constructed?** New mobile office trailers will be delivered and set at the site.
16. **Explain which buildings or what portion of buildings will be used in the operation:** The two new mobile office trailer will be used by SCE's Transmission and Distribution organization for construction and maintenance operations.
17. **Will any outdoor lighting or an outdoor sound amplification system be used?** SCE will install yard lighting (approximately a 25 foot tall pole with mounted lights). No outdoor sound amplification system will be used.

- 18. Landscaping or fencing proposed?** There is a proposed 8-foot chain link fence to be installed along the perimeter of the proposed laydown yard.
- 19. Any other information that will provide a clear understanding of the project operation:** SCE needs laydown yard space in the Shaver Lake area to meet our future project demand and O&M needs. For information regarding the construction of the laydown yard please see Construction Description Document.
- 20. Identify all Owners, Officers and/or Board Members for each application submitted:** See attached excerpt from SCE's 2018 annual report noting SCE's Officers and Board of Directors. As a public company all this information is available on our website, www.edison.com.



Shaver Lake Laydown Yard
41694 Dinkey Creek Road
Shaver Lake, CA 93664
August 14, 2019

Site/Project Information
Unclassified Conditional Use Permit #3639 (Rev. 1)
APN #120-260-10U

Construction Description for Proposed Laydown Yard

The Shaver Lake laydown yard project description is based on planning level assumptions for the construction of the laydown yard. The construction of the laydown yard is estimated to take approximately four months with work being performed concurrently. The following is a high level schedule, construction description and list of equipment and durations that may be used during the construction of the yard.

Schedule:

- 2 months – Grading
- 1 month – Concrete work, Utilities (power, communication, water, septic), Fence and gates
- 2 weeks – Set construction trailers and stairs/ramp
- 2 weeks – Construction Site Clean up

Laydown Yard Construction description:

The site would be prepared by clearing existing vegetation around the construction site. The site would be graded in accordance with the approved grading plans. The permanent disturbance area will be approximately 2.62 acres. The final site grading and drainage would be subject to the conditions of the grading permit obtained from the County of Fresno.

Once the site is graded the laydown yard improvement will be constructed. The fence and gate will be installed, the power, communication, water and septic will be installed and the class II aggregate will be spread and compacted on the yard surface. The concrete pads and ramp will be formed and concrete will be poured and finished.

The laydown yard driveway will be built from an existing paved road. The new driveway into the yard will be finished concrete over a compacted sub-grade and road base aggregate. This driveway would require the improvement of approximately 218 feet by 25 feet off the existing access road north of Dinkey Creek Road.

The construction office trailers will be transported to the laydown yard and assembled, they will be placed on foundation piers and anchored to the pad. The utilities will be connected to the trailer. The ADA compliant deck, stairs and access ramp will be installed and anchored to the pad.

The following estimates are for the ground surface improvements:

- Cut/Fill Soil Quantities:
 - Cut 5,392 Cubic Yards
 - Fill 5,696 Cubic Yards
 - 304 Excessive Cubic Yards
- Yard Rock Surfacing (Class II aggregate base)
 - 2,380 Cubic Yards
- Concrete Quantities
 - 235 Cubic Yards
- Fence (includes gates)
 - 1392 Liner Feet
- Trailers
 - 640 Square Feet
- Conex Box area
 - 320 Square Feet
- Trash/Recycle Area
 - 480 Square Feet

Construction would be performed by either SCE construction crews or its contractors. Contractor construction personnel would be managed by SCE construction management personnel. SCE anticipates a total of approximately 14 construction personnel working on any given day. SCE anticipates that crews would work concurrently whenever possible; however, the estimated deployment and number of crew members would be dependent upon county permitting, material availability and construction scheduling. The following is an estimate for construction equipment and durations that will be used during construction.

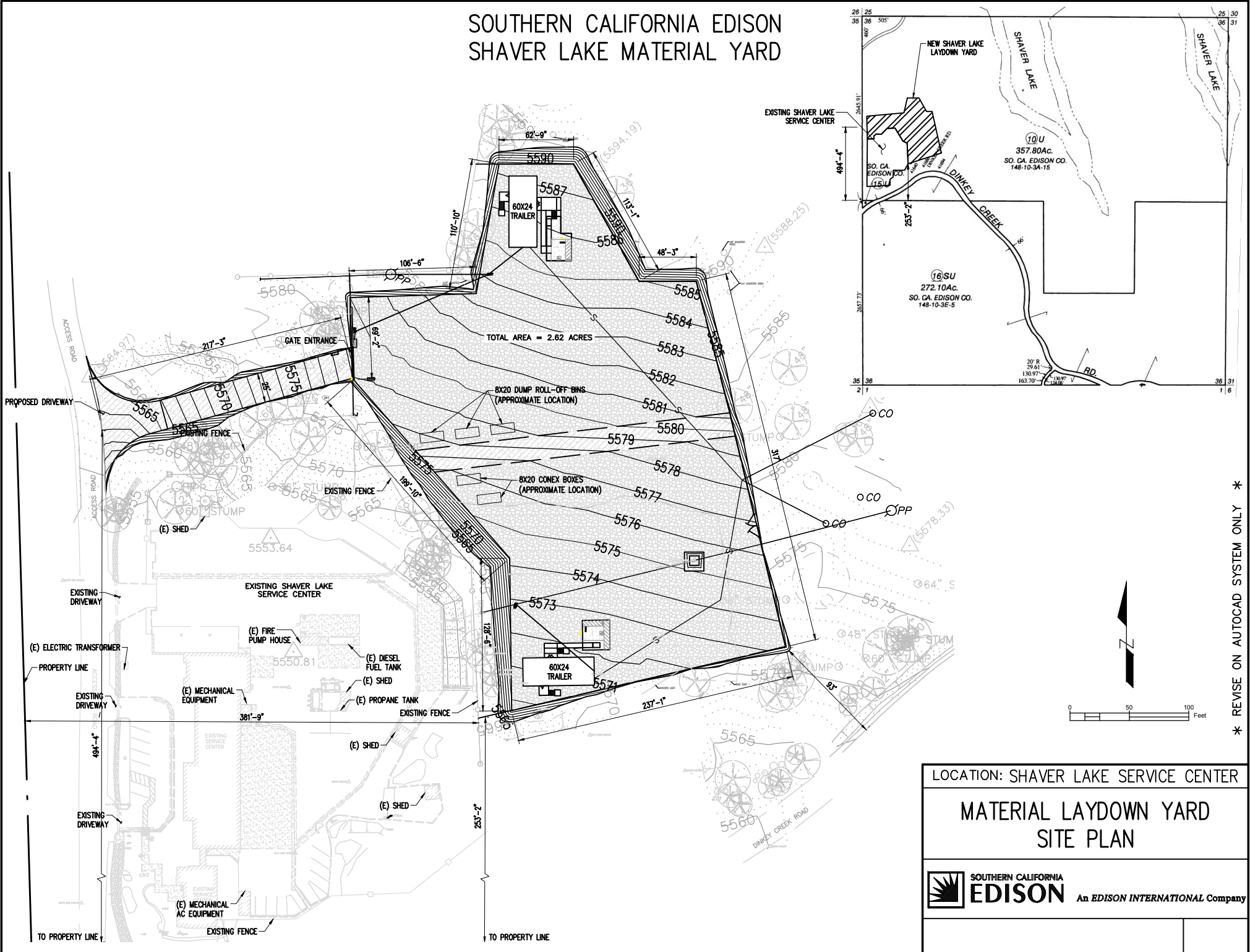
**Table 1:
Construction Equipment and Personnel Use Estimations**

Activity and number of Personnel	Number of Work Weeks	Equipment and Quantity	Duration of Use (Hours/Day)
Grading (6 people)	8	2-Dozer 350 HP (Diesel)	8
		1-Excavator 350 HP (Diesel)	8
		1-Loader 350 HP (Diesel)	8
		1-Scraper 350 HP (Diesel)	8

		1-Grader 350 HP (Diesel) 1-Water Truck 350 HP (Diesel) 1-Vibitory Compactor 300 HP (Diesel) 1-Tool Truck 300 HP (Gasoline) 1-Pickup 4X4 300 HP (Gasoline)	8 8 8 3 8
Fencing and gates (4 people)	2	1-Bobcat 70 HP (Diesel) 1-Flatbed Truck 300 HP (Gasoline) 1-Crewcab Truck 300 HP (Gasoline)	8 2 4
Concrete and utilities (6 people)	4	1-Excavator 350 HP (Diesel) 1-Backhoes 200 HP (Diesel) 1-10 cu. yd. Dump truck 350 HP (Diesel) 1-Skip Loader 350 HP (Diesel) 1-Water Truck 350 HP (Diesel) 1-Bobcat Skid Steer 70 HP (Diesel) 1-Tool Truck 300 HP (Gasoline) 2-10 cu. yd. Concrete Trucks 425 HP (Diesel)	8 8 8 8 6 3 8
Construction Site Clean Up (4 people)	2	1-Bobcat 70 HP (Diesel) 1-Flatbed Truck 300 HP (Gasoline) 1-Crewcab Truck 300 HP (Gasoline)	8 2 4

During construction site cleanup all construction materials and debris would be removed from the area and recycled or properly disposed of at an off-site disposal facility in accordance with all applicable laws. SCE would conduct a final inspection to ensure that cleanup activities are successfully completed.

SOUTHERN CALIFORNIA EDISON SHAVER LAKE MATERIAL YARD



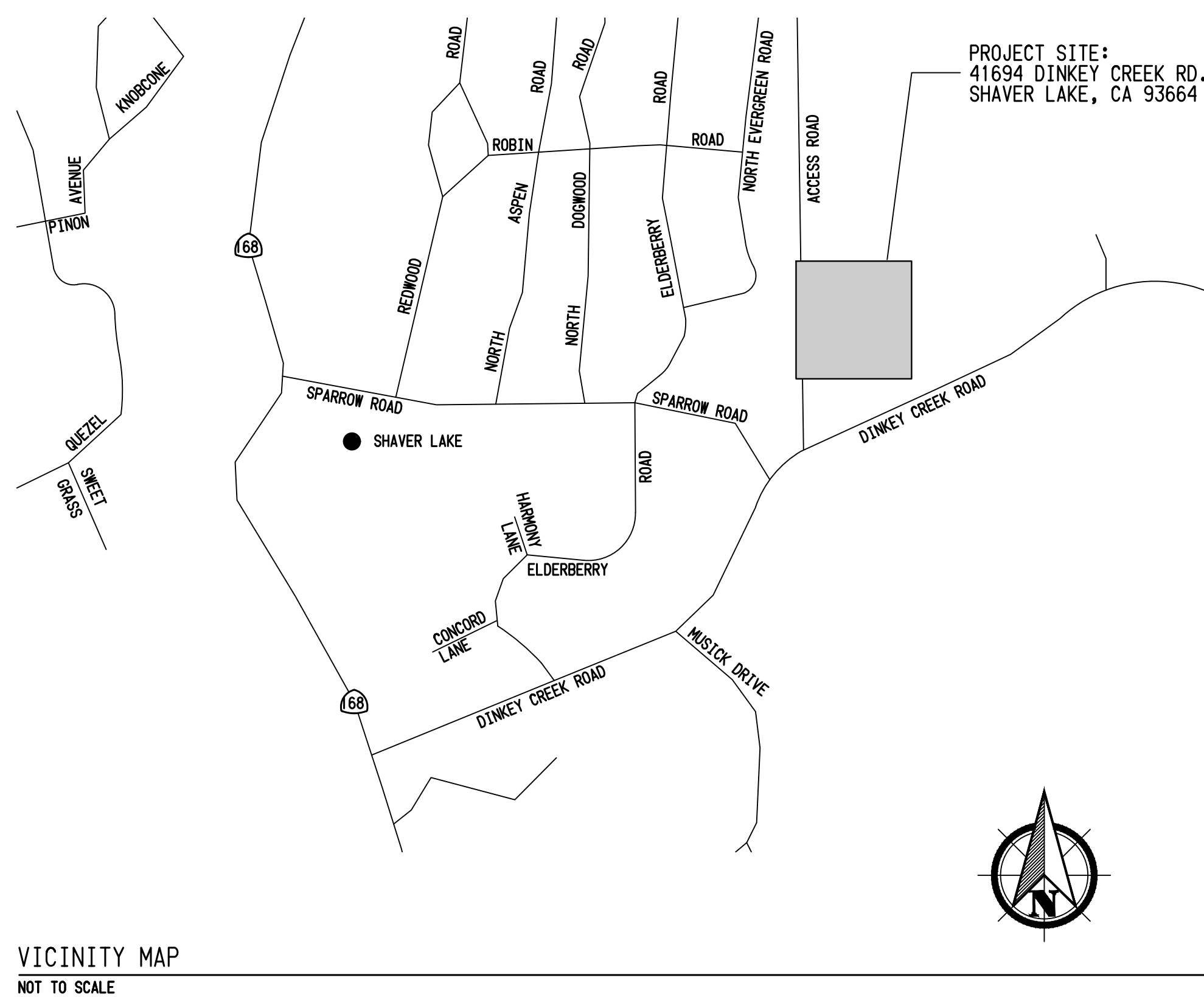
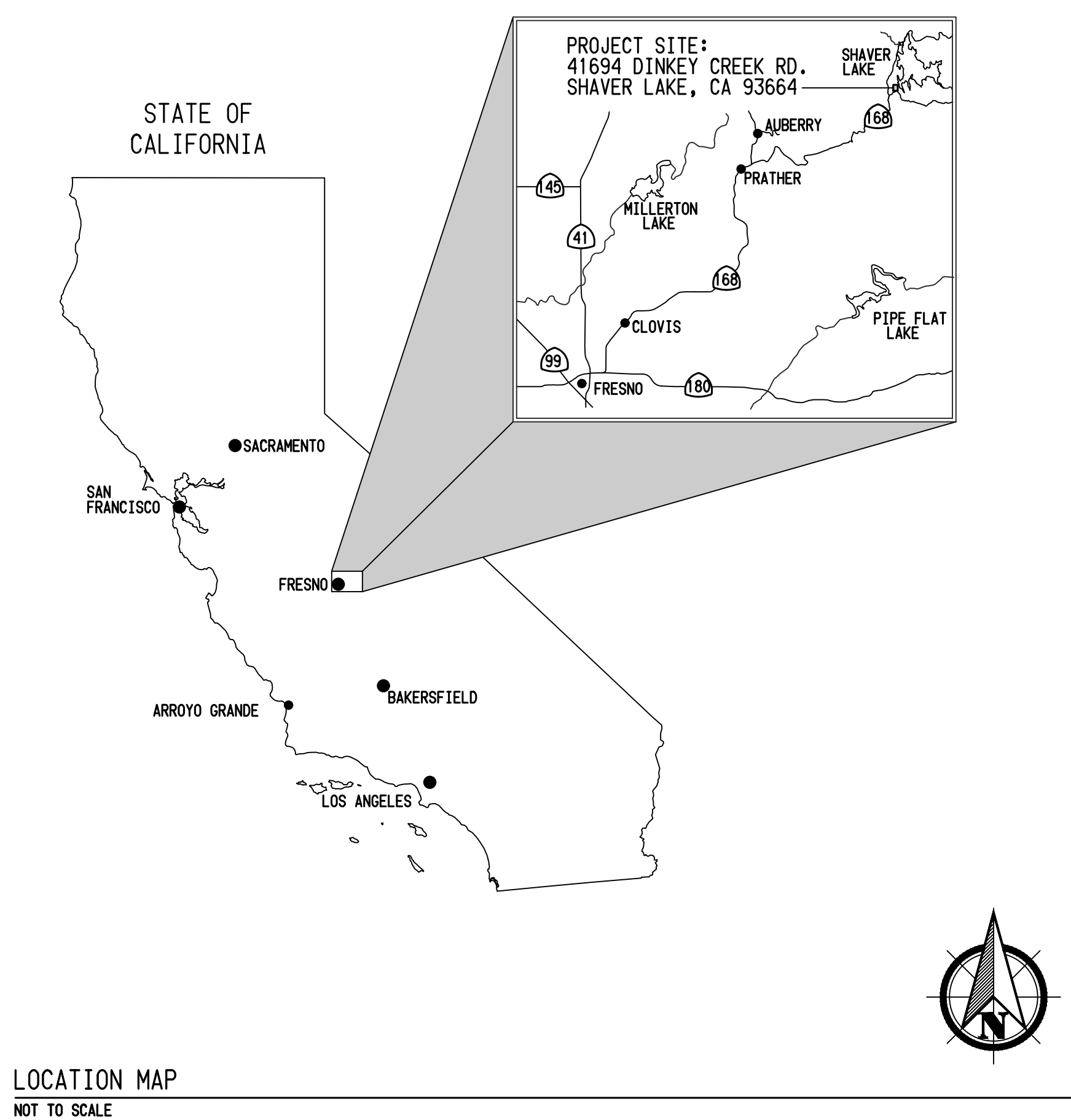
* REVISE ON AUTOCAD SYSTEM ONLY *

LOCATION: SHAVER LAKE SERVICE CENTER
**MATERIAL LAYDOWN YARD
 SITE PLAN**





SHAVER LAKE SERVICE CENTER LAYDOWN YARD



GRADING AND DRAINAGE NOTES

- THE WORK ENRAGED HEREIN SHALL BE IN ACCORDANCE WITH THE APPROPRIATE PROVISIONS OF CHAPTER 18, CHAPTER 33 AND APPENDIX J OF 2013 CALIFORNIA BUILDING CODE AND CALIFORNIA RESIDENTIAL CODE AS ADOPTED AND AMENDED BY CHAPTER 15.28 OF THE FRESNO COUNTY ORDINANCE CODE.
- A GRADING PERMIT OR VOUCHER SHALL BE OBTAINED FROM THE COUNTY OF FRESNO PUBLIC WORKS AND PLANNING DEPARTMENT, DEVELOPMENT ENGINEERING SECTION PRIOR TO ANY GRADING AND PAVING WORK FOR THIS PROJECT.
- A NOTICE OF INTENT (NOI) AND STORM WATER POLLUTION PLAN (SWPPP) ARE REQUIRED TO BE FILED WITH STATE WATER RESOURCES CONTROL BOARD BEFORE THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES DISTURBING 1.0 ACRE OR MORE OF AREA. COPIES OF NOI AND SWPPP SHALL BE PROVIDED TO DEVELOPMENT ENGINEERING PRIOR TO THE APPROVAL OF THESE PLANS.
- MAXIMUM CUT AND FILL SLOPES SHALL BE 2H:1V OR AS ALLOWED PER AN APPROVED AND FILED SOILS REPORT.
- THE GROUND AROUND THE STRUCTURE SHALL BE SLOPED TWO PERCENT (2%) FOR A DISTANCE OF FIVE FEET (5') TO PROVIDE DRAINAGE AWAY FROM THE STRUCTURE OR AS APPROVED BY THE BUILDING OFFICIAL.
- COMPACTION REPORTS ARE REQUIRED TO BE SUBMITTED TO DEVELOPMENT ENGINEERS FOR ALL FILLS.
- A "HOLD ON OCCUPANCY" WILL BE IN EFFECT UNTIL SUCH TIME AS THE DEVELOPMENT IS CERTIFIED "AS-BUILT" BY THE ENGINEER OF RECORD.

TABLE OF CONTENTS

TITLE	SHEET NO.	DRAWING NO.
COVER SHEET	TO.0	
TOPOGRAPHIC SURVEY AND DEMOLITION PLAN	CO.0	
TOPOGRAPHIC SURVEY AND DEMOLITION PLAN	CO.1	
OVERALL SITE PLAN	CO.2	
SITE AND GRADING PLAN	C1.0	
SITE AND GRADING PLAN	C1.1	
SITE DETAILS	C2.0	
SITE DETAILS	C2.1	
AUTOMATIC AND SWING GATE DETAILS	C2.2	
PARTIAL ELECTRICAL SITE PLAN (SUBMITTED W/ BUILDING PERMIT)	E1.0	
PARTIAL ELECTRICAL SITE PLAN (SUBMITTED W/ BUILDING PERMIT)	E1.1	

GENERAL NOTES

- THE WORK ENRAGED HEREIN SHALL BE IN ACCORDANCE WITH THE APPROPRIATE PROVISIONS OF THE SPECIFICATIONS ENTITLED "IMPROVEMENT STANDARDS, COUNTY OF FRESNO, STATE OF CALIFORNIA, OCTOBER, 1966 AND AS AMENDED, INSOFAR AS THE SAME MAY APPLY.
- THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT OF PUBLIC WORKS AND PLANNING OF THE COUNTY OF FRESNO AT (559) 600-4022 AND 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- ALL WORK INSTALLED SHALL MEET OR EXCEED COUNTY IMPROVEMENT STANDARDS. WHERE WORK PROPOSED IS NOT ADDRESSED WITH THESE STANDARDS, THE CONTRACTOR SHALL DEFER THE LATEST ADOPTED VERSION OF THE STATE STANDARD DRAWINGS AND SPECIFICATIONS FOR COMPLIANCE.
- AN ENCROACHMENT PERMIT SHALL BE OBTAINED FROM THE ROAD MAINTENANCE & OPERATIONS DIVISION OF PUBLIC WORKS FOR ANY WORK WITHIN THE RIGHT-OF-WAY. CALL (559) 600-4247 FOR THE PERMIT ENGINEER AT LEAST 72 HOURS PRIOR TO COMMENCEMENT OF ANY WORK. ALL WORK PERFORMED IN THE COUNTY RIGHT-OF-WAY SHALL BE IN COMPLIANCE WITH ROAD MAINTENANCE & OPERATION DIVISION REQUIREMENTS.
- THE WORK ENRAGED HEREIN SHALL NOT BE COMMENCED UNTIL THE OWNER OF THE PROPERTY OR THE CONTRACTOR HAS FIRST BEEN PROVIDED AN INQUIRY IDENTIFICATION NUMBER FOR SUB-SURFACE INSTALLATIONS AS SPECIFIED IN SECTION 4216 OF THE GOVERNMENT CODE.
- BEFORE COMMENCING WORK, THE CONTRACTOR SHALL NOTIFY ALL UTILITY AUTHORITIES OR UTILITY COMPANIES HAVING POSSIBLE INTEREST IN THE WORK OF THE CONTRACTOR'S INTENTION TO EXCAVATE PROXIMATE TO EXISTING FACILITIES AND THE CONTRACTOR SHALL VERIFY THE LOCATION OF ANY UTILITIES IN THE WORK AREA. THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (U.S.A.) TWO (2) DAYS PRIOR TO BEGINNING ANY EXCAVATION.
- THE CONTRACTOR SHALL OBTAIN WRITTEN AUTHORIZATION FROM ANY PROPERTY OWNER GIVING HIM PERMISSION TO ENTER HIS PROPERTY FOR THE PURPOSES OF CONSTRUCTING THE IMPROVEMENTS DELINEATED ON THE PLANS AND TRANSMISSIONS THERE TO. THE CONTRACTOR SHALL PROVIDE THE COUNTY WITH A COPY PRIOR TO START OF ANY WORK.
- WHERE IMPROVEMENTS ARE PROPOSED THAT DO NOT MEET COUNTY STANDARDS, A SUBMITTAL SHALL BE PROVIDED TO DEVELOPMENT ENGINEERING AND/OR RESOURCES AND APPROVAL GRANTED PRIOR TO INSTALLATION.
- DIRT OR DEBRIS TRACKED ONTO EXISTING COUNTY ROADS FROM THIS PROJECT SHALL BE CLEANED OFF AT THE END OF EACH WORKING DAY TO THE SATISFACTION OF THE COUNTY INSPECTOR.
- THE COSTS OF ALL TESTING REQUIRED FOR ACCEPTANCE OF WORK SHALL BE FULLY BORNE BY THE APPLICANT.
- ALL WORK PERFORMED "AT-RISK" WITHOUT APPROVAL OF THESE PLANS, OR ANY REVISIONS THEREAFTER INSPECTION OR PROPER PERMITS FROM THE COUNTY IS SUBJECT TO REVISION, EXPOSURE, REMOVAL AND REPLACEMENT AT THE APPLICANT'S COST.
- TEMPORARY EROSION CONTROL SHALL BE PROVIDED WITH THE INSTALLATION OF STRAW BALES, SILT FENCES OR OTHER MEANS AGREEABLE TO THE COUNTY OF FRESNO
- ALL WORK PROPOSED NOT SPECIFICALLY IDENTIFIED OR DISCUSSED IN THE COUNTY IMPROVEMENT STANDARDS SHALL BE IN COMPLIANCE WITH THE LATEST VERSION OF THE CALTRANS STANDARD PLANS AND SPECIFICATIONS.
- PERMANENT EROSION CONTROL SHALL BE STRAW-AND-SEED, JUTE MESH NETTING, HYDRO-MULCH, OR SOME TYPE OF LANDSCAPING TO PREVENT LOSS OF MATERIAL. ALL WORK PROPOSED SHALL COMPLY WITH SECTIONS 20 AND 21 OF THE CALTRANS STANDARD SPECIFICATIONS.
- THE WORK ENRAGED HEREIN SHALL NOT BE APPROVED UNTIL SUCH TIME AS THE OWNER OF THE PROPERTY OR THE CONTRACTOR HAS COMPLIED WITH ALL APPLICABLE REQUIREMENTS AS SPECIFIED IN SECTION 4290 OF THE PUBLIC RESOURCES CODE, AS APPLICABLE

CONSULTANTS

ENGINEER / DESIGNER	OWNER REPRESENTATIVE
BLAIR, CHURCH & FLYNN PALO ALTO, CA, PE, QSD 451 CLOVIS AVENUE, SUITE 200 CLOVIS, CALIFORNIA 93612 TEL (559) 326-1400 FAX (559) 326-1500	SOUTHERN CALIFORNIA EDISON PASCUAL GARCIA 801 W CIPRESA AVENUE SAN DIMAS, CALIFORNIA 91773 TEL (909) 394-2876

PROJECT DATA

PROJECT SCOPE: CONSTRUCTION OF A NEW LAYDOWN YARD, AND (2) 24' x 60' TEMPORARY TRAILERS FOR SOUTHERN CALIFORNIA EDISON LOCATED AT SHAVER LAKE SERVICE CENTER IN SHAVER, CA.	
GENERAL DATA:	
SITE ADDRESS:	41694 DINKEY CREEK RD., SHAVER LAKE CA, 93664
APN:	120-260-10U
ZONING:	RC40
JURISDICTION:	FRESNO COUNTY
TOTAL SITE AREA (DISTURBED):	2.62 ACRE
CUT:	4078.30 CU. YD.
FILL:	7177.22 CU. YD.
IMPORT:	3098.93 CU. YD.
FLOOD ZONE:	ZONE X
FIRM MAP:	06019C0725H
CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONTRACTOR ASSUMES SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE PERFORMANCE OF WORK, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, ON A CONTINUOUS BASIS NOT LIMITED TO NORMAL WORKING HOURS. CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD HARMLESS BLAIR, CHURCH & FLYNN CONSULTING ENGINEERS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF BLAIR, CHURCH & FLYNN CONSULTING ENGINEERS.	

PLANS PREPARED BY:

**Blair,
Church
& Flynn**
CONSULTING ENGINEERS

451 Clovis Avenue, Suite 200
Clovis, California 93612
Tel (559) 326-1400
Fax (559) 326-1500

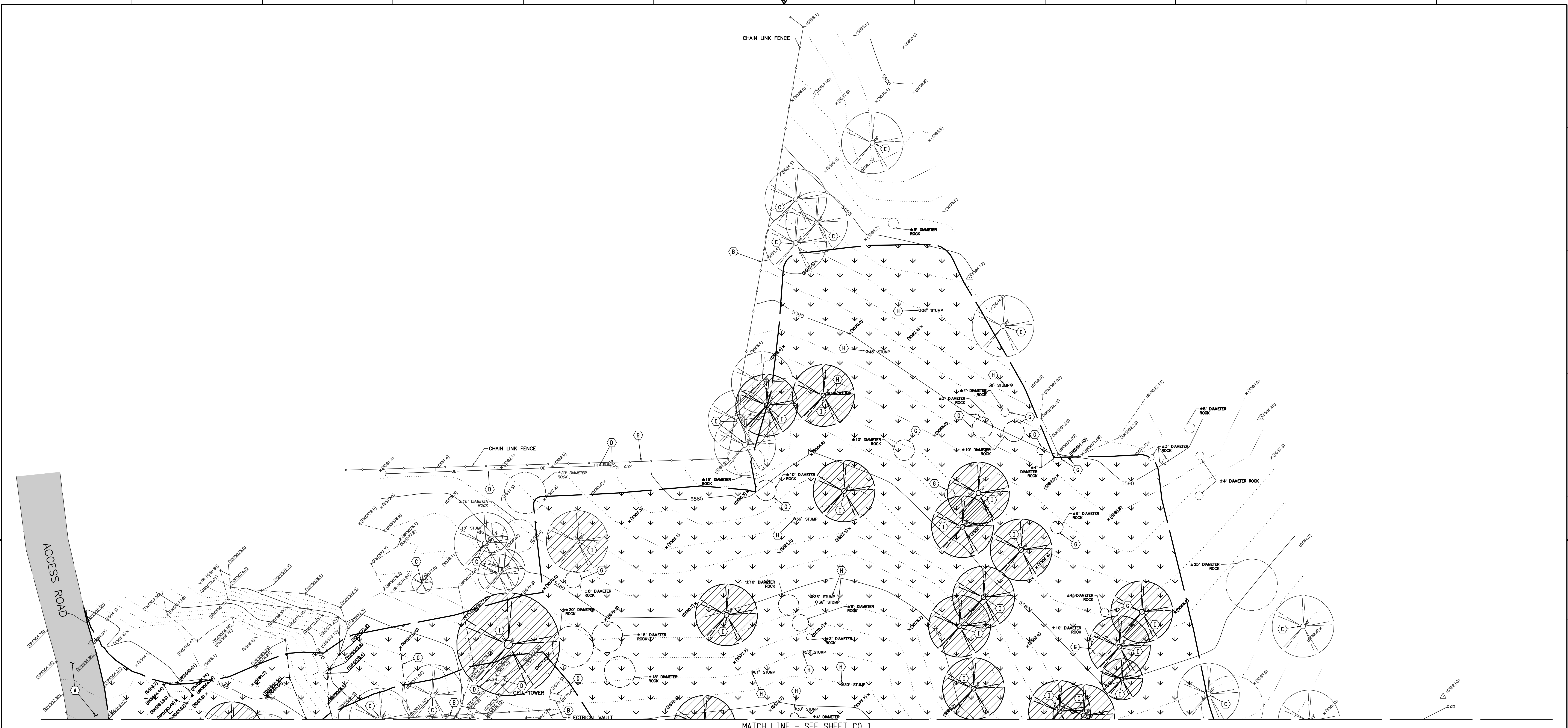


6/20/2019
DATE SIGNED:

LOCATION: SHAVER LAKE, CALIFORNIA	SHEET NO.
SHAVER LAKE SERVICE CENTER LAYDOWN YARD COVER SHEET	TO.0
SCALE: N/A	1 OF 11 SHTS.



NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	
	BACKCHECK SUBMITTAL	01-30-19					PN	DM		PN												
	BACKCHECK SUBMITTAL	05-30-19					PN	DM		PN												



TOPOGRAPHIC LEGEND

(335.21)	EXISTING ELEVATION	TREE: SPREAD SHOWN GRAPHICALLY AND TRUNK DIAMETER AS SHOWN
AC	ASPHALTIC CONCRETE	AC PAVING IMPROVEMENTS
CE	CONCRETE EDGE	EXISTING BUILDING
EP	EDGE OF PAVEMENT	CONCRETE IMPROVEMENTS
GB	GRADE BREAK	CHAIN LINK FENCE
RK	ROCK	WOOD FENCE
TOE	TOE OF SLOPE	EDGE OF ASPHALT CONCRETE PAVEMENT
TOP	TOP OF SLOPE	GRADE BREAK
VGR	VALLEY GUTTER	ROCK
△ 312.55	SURVEY CONTROL POINT	MAJOR CONTOUR
---	GUY WIRE	MINOR CONTOUR
⊕	LIGHT POLE	OVERHEAD ELECTRIC
⊕	MANHOLE	SEWER LINE; SIZE AS NOTED
⊕	POWER POLE	
○	SEWER CLEANOUT	
—	SLOPE	
⊕ 12"	STUMP; DIAMETER AS SHOWN	
○ TR	TELEPHONE RISER / PEDESTAL	

DEMOLITION LEGEND

(A)	PROTECT EXISTING ASPHALT CONCRETE PAVEMENT TO REMAIN
(B)	PROTECT EXISTING CHAIN LINK FENCE TO REMAIN
(C)	PROTECT EXISTING TREE AND ROOTS TO REMAIN
(D)	PROTECT EXISTING UTILITY TO REMAIN
(E)	REMOVE AND LAWFULLY DISPOSE OF ROCK
(G)	REMOVE AND LAWFULLY DISPOSE OF STUMP
(H)	REMOVE AND LAWFULLY DISPOSE OF TREE, ROOTBALL, AND ISOLATED ROOTS GREATER THAN 1/2 INCH IN DIAMETER
(I)	REMOVE EXISTING IMPROVEMENTS AS NECESSARY TO CONSTRUCT NEW IMPROVEMENTS SHOWN ON THESE PLANS UNLESS OTHERWISE NOTED ON THE PLAN. THE REMOVAL OF IMPROVEMENTS MUST BE COORDINATED WITH ALL PLAN SHEETS. CONTRACTOR MUST ALSO COORDINATE REMOVAL OF IMPROVEMENTS WITH UTILITY AGENCIES. PROTECT ALL IMPROVEMENTS NOT DESIGNATED FOR REMOVAL. SEE NOTE 1
(T)	TREE AND ROOTS TO BE REMOVED

DEMOLITION NOTES

- THE "LIMIT OF DEMOLITION" SHOWN IS APPROXIMATE AND IS GENERALLY CONSIDERED TO BE THE MINIMUM REMOVAL REQUIREMENTS. CONTRACTOR MUST COORDINATE AS NOTED IN THE LEGEND. REFER TO THE GEOTECHNICAL REPORT FOR SITE PREPARATION REQUIREMENTS.
- CONTRACTOR SHALL LEGALLY DISPOSE OF ALL DEMOLISHED MATERIALS OFF SITE.
- CONTRACTOR SHALL PROTECT ALL EXISTING UTILITY IMPROVEMENTS NOT SPECIFICALLY DESIGNATED FOR REMOVAL.
- THE ON-SITE UNDERGROUND UTILITIES SHOWN ON THIS SHEET ARE AT AN APPROXIMATE LOCATION. THE EXTENT, LOCATIONS AND SIZES ARE UNKNOWN. THE CONTRACTOR SHALL POTHOLE TO LOCATE AND VERIFY THE UNDERGROUND UTILITY LINES PRIOR TO REMOVAL.
- CONTRACTOR TO PROTECT AND PRESERVE IN PLACE ANY FOUND SURVEY MONUMENTS. ANY MONUMENTS DISTURBED SHALL BE RESET BY A LICENSED SURVEYOR AND THE APPROPRIATE PAPERWORK FILED WITH THE CITY OR COUNTY.
- ALL HAZARDOUS MATERIALS ENCOUNTERED DURING SITE DEMOLITION SHALL BE REMEDIATED AND DISPOSED OF PER STATE AND EPA REQUIREMENTS.
- CONTRACTOR SHALL CONTACT AND COORDINATE WITH ALL UTILITY AGENCIES PRIOR TO THE START OF ANY DEMOLITION OR CONSTRUCTION.
- ANY EXISTING UTILITIES AND/OR IMPROVEMENTS THAT BECOME DAMAGED DURING CONSTRUCTION SHALL BE COMPLETELY RESTORED TO THE SATISFACTION OF THE OWNER AND AGENCY HAVING AUTHORITY, AT THE CONTRACTOR'S SOLE EXPENSE.

SURVEY NOTES

THIS TOPOGRAPHIC SURVEY LOCATES SPECIFIC PHYSICAL FEATURES OF THE SITE AND THEIR ELEVATION AS DETERMINED NECESSARY BY THE PROJECT ENGINEER. IT IS NOT A COMPLETE TOPOGRAPHIC SURVEY OF THE SITE. THE INFORMATION SHOWN REFLECTS THE DATA OBTAINED BY FIELD SURVEY CONDUCTED ON MAY 17, 2018.

BENCHMARK

OCCUPIED EXISTING CONTROL.
 TEM = CONTROL POINT NO.1
 ELEVATION = 5550.00' ASSUMED DATUM

REFERENCE DRAWINGS	NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.

BACKCHECK SUBMITTAL	01-30-19	PN	DM	PN
BACKCHECK SUBMITTAL	05-30-19	PN	DM	PN

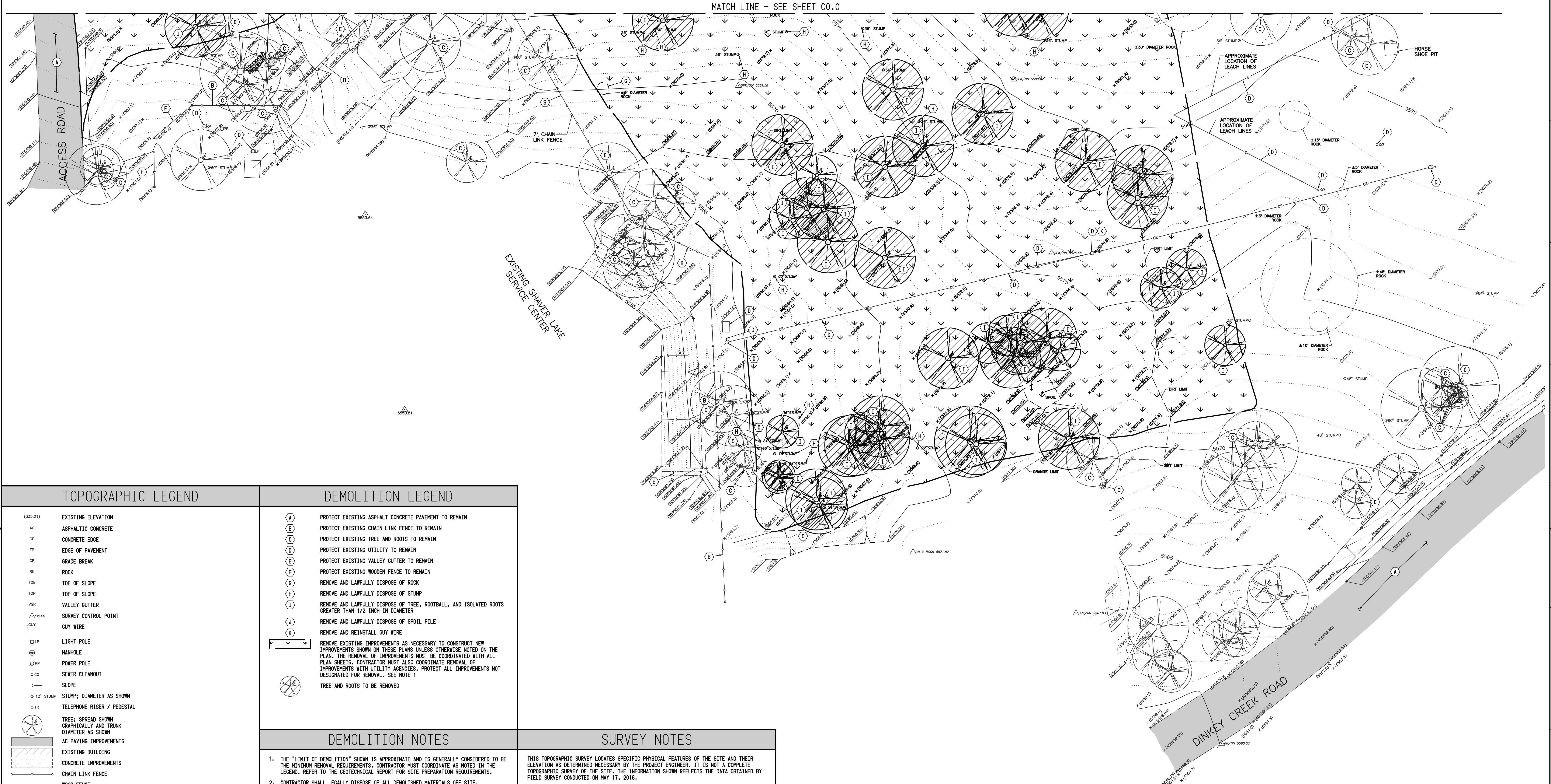
REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.

PLANS PREPARED BY:
Blair, Church & Flynn
 CONSULTING ENGINEERS
 465 Clovis Avenue, Suite 200
 Clovis, California 93222
 Tel: (559) 328-1400
 Fax: (559) 328-1800

LOCATION: SHAVER LAKE, CALIFORNIA	SHEET NO. CO.0
SHAVER LAKE SERVICE CENTER LAYDOWN YARD	SCALE: 1" = 20'
TOPOGRAPHIC SURVEY AND DEMOLITION PLAN	2 OF 11 SHTS.
EDISON An EDISON INTERNATIONAL Company	

Drawing: P:\218-00018\project\dwgs\118000_19.dwg, Co.0 Topographic Survey and Demolition Plan - BCF.ctb
 User: jrg, Operator: jrg, Date: 5/17/2019, Time: 1:11:00 PM

MATCH LINE - SEE SHEET CO.0



TOPOGRAPHIC LEGEND

- (555.21) EXISTING ELEVATION
- AC ASPHALTIC CONCRETE
- CE CONCRETE EDGE
- EP EDGE OF PAVEMENT
- GB GRADE BREAK
- RK ROCK
- TOE TOE OF SLOPE
- TOP TOP OF SLOPE
- VGR VALLEY GUTTER
- △110.00 SURVEY CONTROL POINT
- △110.00 GUY WIRE
- ⊕ LIGHT POLE
- ⊕ MANHOLE
- ⊕ POWER POLE
- ⊕ SEWER CLEANOUT
- SLOPE
- ⊕ 12" STUMP STUMP; DIAMETER AS SHOWN
- ⊕ TR TELEPHONE RISER / PEDESTAL
- TREE; SPREAD SHOWN GRAPHICALLY AND TRUNK DIAMETER AS SHOWN
- AC PAVING IMPROVEMENTS
- EXISTING BUILDING
- CONCRETE IMPROVEMENTS
- CHAIN LINK FENCE
- WOOD FENCE
- EDGE OF ASPHALTIC CONCRETE PAVEMENT
- GRADE BREAK
- ROCK
- 5565 MAJOR CONTOUR
- MINOR CONTOUR
- OVERHEAD ELECTRIC
- 1/2" SEWER LINE; SIZE AS NOTED

DEMOLITION LEGEND

- (A) PROTECT EXISTING ASPHALT CONCRETE PAVEMENT TO REMAIN
- (B) PROTECT EXISTING CHAIN LINK FENCE TO REMAIN
- (C) PROTECT EXISTING TREE AND ROOTS TO REMAIN
- (D) PROTECT EXISTING UTILITY TO REMAIN
- (E) PROTECT EXISTING VALLEY GUTTER TO REMAIN
- (F) PROTECT EXISTING WOODEN FENCE TO REMAIN
- (G) REMOVE AND LAWFULLY DISPOSE OF ROCK
- (H) REMOVE AND LAWFULLY DISPOSE OF STUMP
- (I) REMOVE AND LAWFULLY DISPOSE OF TREE, ROOTBALL, AND ISOLATED ROOTS GREATER THAN 1/2 INCH IN DIAMETER
- (J) REMOVE AND LAWFULLY DISPOSE OF SPOIL PILE
- (K) REMOVE AND REINSTALL GUY WIRE
- REMOVE EXISTING IMPROVEMENTS AS NECESSARY TO CONSTRUCT NEW IMPROVEMENTS SHOWN ON THESE PLANS UNLESS OTHERWISE NOTED ON THE PLAN. THE REMOVAL OF IMPROVEMENTS MUST BE COORDINATED WITH ALL PLAN SHEETS. CONTRACTOR MUST ALSO COORDINATE REMOVAL OF IMPROVEMENTS WITH UTILITY AGENCIES. PROTECT ALL IMPROVEMENTS NOT DESIGNATED FOR REMOVAL. SEE NOTE 1.
- ⊗ TREE AND ROOTS TO BE REMOVED

DEMOLITION NOTES

1. THE "LIMIT OF DEMOLITION" SHOWN IS APPROXIMATE AND IS GENERALLY CONSIDERED TO BE THE MINIMUM REMOVAL REQUIREMENTS. CONTRACTOR MUST COORDINATE AS NOTED IN THE LEGEND. REFER TO THE GEOTECHNICAL REPORT FOR SITE PREPARATION REQUIREMENTS.
2. CONTRACTOR SHALL LEGALLY DISPOSE OF ALL DEMOLISHED MATERIALS OFF SITE.
3. CONTRACTOR SHALL PROTECT ALL EXISTING UTILITY IMPROVEMENTS NOT SPECIFICALLY DESIGNATED FOR REMOVAL.
4. THE ON-SITE UNDERGROUND UTILITIES SHOWN ON THIS SHEET ARE AT AN APPROXIMATE LOCATION. THE EXTENT, LOCATIONS AND SIZES ARE UNKNOWN. THE CONTRACTOR SHALL POT-HOLE TO LOCATE AND VERIFY THE UNDERGROUND UTILITY LINES PRIOR TO REMOVAL.
5. CONTRACTOR TO PROTECT AND PRESERVE IN PLACE ANY FOUND SURVEY MONUMENTS. ANY MONUMENTS DISTURBED SHALL BE RESET BY A LICENSED SURVEYOR AND THE APPROPRIATE PAPERWORK FILLED WITH THE CITY OR COUNTY.
6. ALL HAZARDOUS MATERIALS ENCOUNTERED DURING SITE DEMOLITION SHALL BE REMEDIATED AND DISPOSED OF PER STATE AND EPA REQUIREMENTS.
7. CONTRACTOR SHALL CONTACT AND COORDINATE WITH ALL UTILITY AGENCIES PRIOR TO THE START OF ANY DEMOLITION OR CONSTRUCTION.
8. ANY EXISTING UTILITIES AND/OR IMPROVEMENTS THAT BECOME DAMAGED DURING CONSTRUCTION SHALL BE COMPLETELY RESTORED TO THE SATISFACTION OF THE OWNER AND AGENCY HAVING AUTHORITY, AT THE CONTRACTOR'S SOLE EXPENSE.

SURVEY NOTES

THIS TOPOGRAPHIC SURVEY LOCATES SPECIFIC PHYSICAL FEATURES OF THE SITE AND THEIR ELEVATION AS DETERMINED NECESSARY BY THE PROJECT ENGINEER. IT IS NOT A COMPLETE TOPOGRAPHIC SURVEY OF THE SITE. THE INFORMATION SHOWN REFLECTS THE DATA OBTAINED BY FIELD SURVEY CONDUCTED ON MAY 17, 2018.

BENCHMARK

OCCUPIED EXISTING CONTROL.
 TBM = CONTROL POINT NO. 1
 ELEVATION = 5550.00' ASSUMED DATUM

Drawing: P:\2018-2019\GIS\product\mgs\018000_rts.dwg; Co.1 Topographic Survey and Demolition Plan - BCF.ctb
 User: jf.Operations; Plot Date: 6/20/2019 11:11:03am

PLANS PREPARED BY:

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 Fax: (509) 528-4300

REGISTERED PROFESSIONAL ENGINEER
 STATE OF CALIFORNIA
 No. 78,395
 Paul R. Flynn

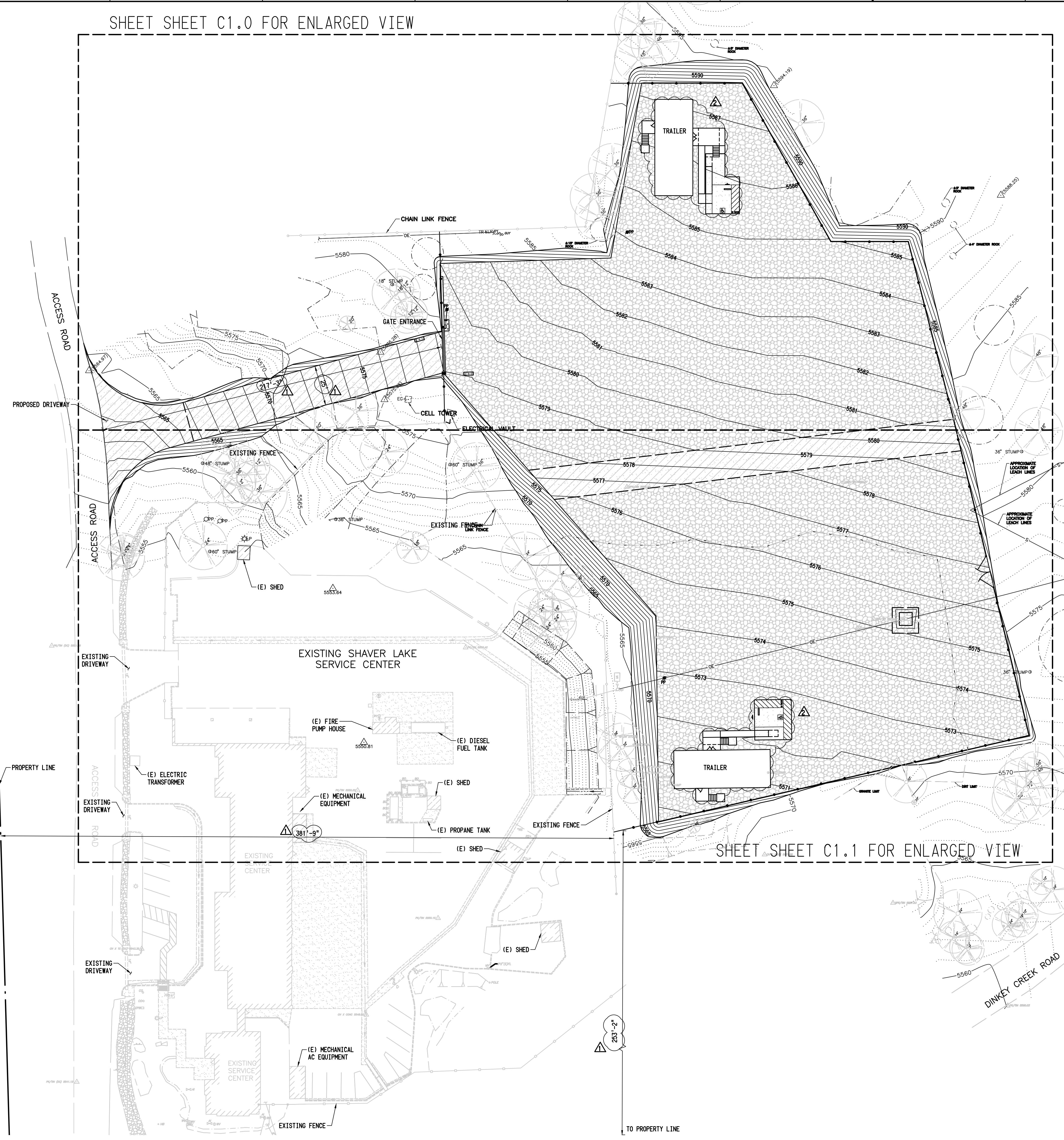
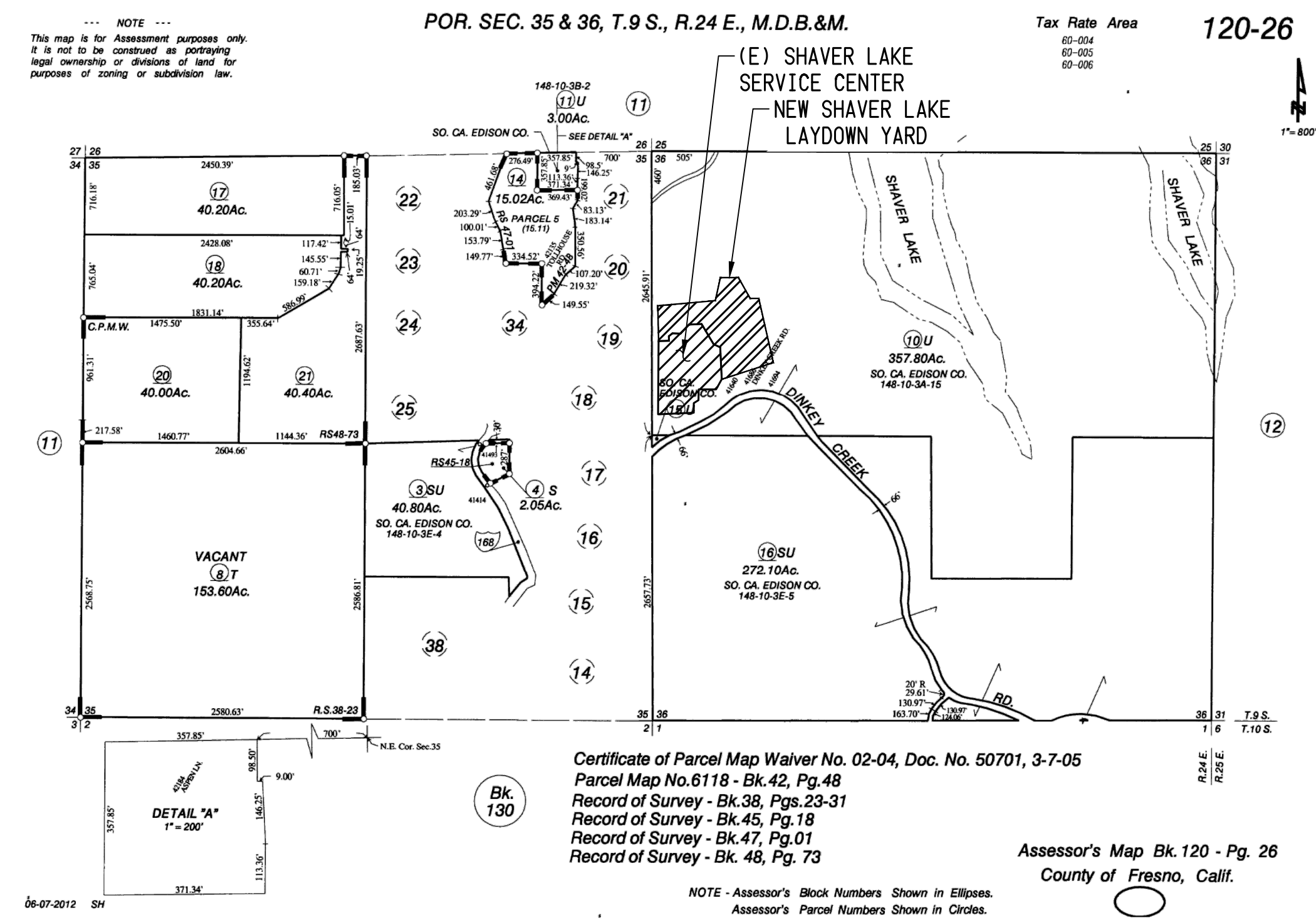
6/20/2019
 UTM 847338

LOCATION: SHAVER LAKE, CALIFORNIA		SHEET NO. CO.1 SCALE: 1" = 20' 3 OF 11 SHTS.
SHAVER LAKE SERVICE CENTER LAYDOWN YARD TOPOGRAPHIC SURVEY AND DEMOLITION PLAN		
An EDISON INTERNATIONAL Company		

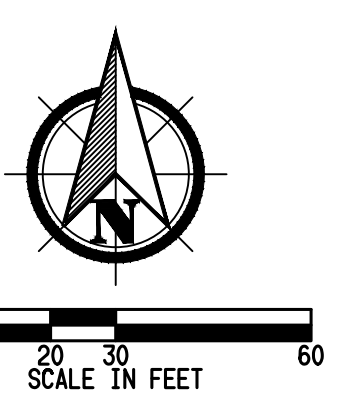
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1	BACKCHECK SUBMITTAL	01-30-19					PN	DM	PN														
2	BACKCHECK SUBMITTAL	05-30-19					PN	DM	PN														

SHEET SHEET C1.0 FOR ENLARGED VIEW

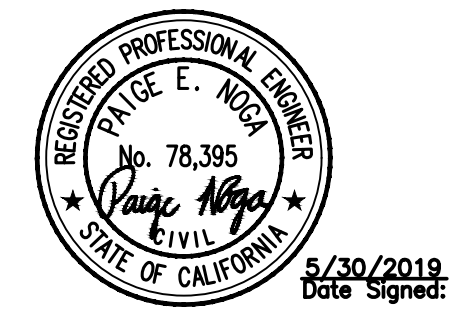
SHEET SHEET C1.1 FOR ENLARGED VIEW



Drawing: P:\218-0200\GIS\repro\cadd\cadd\218000_s102.dwg; CO.2 Overall Site Plan - B05.ctb
 Plot by: operations May 31, 2019 - 11:17am



PLANS PREPARED BY:
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 Fax (509) 238-1500



NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	
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	BACKCHECK SUBMITTAL	05-30-19					PN	DM		PN												

LOCATION: SHAVERS LAKE, CALIFORNIA
 SHAVERS LAKE SERVICE CENTER
 LAYDOWN YARD
 OVERALL SITE PLAN

SHEET NO.
C0.2

SCALE: 1" = 30'
 4 OF 11 SHTS.



ENGINEER'S CERTIFICATE
 THIS PLAN WILL NOT IMPOSE A DRAINAGE, GRADING OR FLOODING HAZARD TO SURROUNDING PROPERTIES.
 Paul Alga
 PAUL ALGA
 HCE 78,395

MATCH LINE - SEE SHEET C1.1

CONSTRUCTION LEGEND

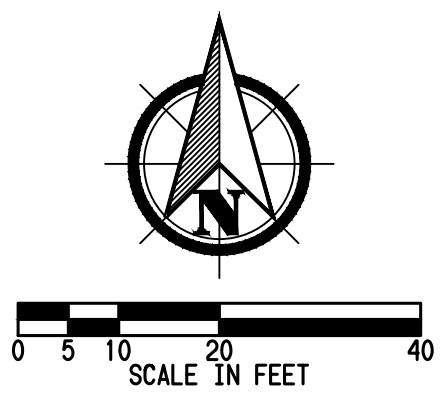
- (A) CONSTRUCT CONCRETE PAVEMENT IMPROVEMENTS PER DETAIL 1 C2.0
 - (B) CONSTRUCT HEAVY DUTY CONCRETE PAVEMENT IMPROVEMENTS PER DETAIL 4 C2.0
 - (C) CONSTRUCT GRAVEL IMPROVEMENTS PER DETAIL 3 C2.0
 - (D) FURNISH AND INSTALL 8" HIGH CHAIN LINK FENCE PER DETAIL 1 C2.1
 - (E) FURNISH AND INSTALL SLIDE GATE PER DETAIL 7 C2.2
 - (F) FURNISH GATE TRACK FOUNDATION PER DETAIL 5 C2.2
 - (G) NEW POWER POLE AND OVER HEAD ELECTRICAL LINE TO BE INSTALLED BY OTHERS
 - (H) FURNISH AND INSTALL CONCRETE WHEELSTOP PER DETAIL 9 C2.0
 - (I) FURNISH AND INSTALL ACCESSIBLE SIGN PER DETAIL 8 C2.0
 - (J) STEEL RAMP AND STAIRS PER TRAILER PLANS
 - (K) GATE CONTROLLER AND CHAMBERLAIN ELITE SL3000 MOTOR TO BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS
 - (L) KEY PAD TO BE INSTALLED PER DETAIL 4 C2.2
 - (M) SIGN PER DETAIL 2 C2.0 5 C2.0
- CONCRETE IMPROVEMENTS
 HEAVY DUTY CONCRETE IMPROVEMENTS
 GRAVEL IMPROVEMENTS
 NEW CHAIN LINK FENCE

GRADING LEGEND

- C CONCRETE
- FG FINISHED GRADE
- GB GRADE BREAK
- GRA GRAVEL
- P PAVEMENT
- GRA5883.00 NEW FINISHED SURFACE GRADE
- 1.8% DIRECTION OF DRAINAGE
- 5585 MAJOR CONTOURS
- MINOR CONTOURS

GENERAL GRADING AND DRAINAGE NOTES

- THE REQUIREMENTS AND INFORMATION SET OUT BELOW ARE PROVIDED FOR THE CONTRACTOR'S CONFORMANCE AND DO NOT ENCOMPASS ALL PROJECT REQUIREMENTS DESCRIBED BY THE PROJECT PLANS AND SPECIFICATIONS AND/OR APPLICABLE LAWS, REGULATIONS AND/OR BUILDING CODES.
- CONSTRUCTION OF ALL PROJECT SITE IMPROVEMENTS SUBJECT TO ADA ACCESS COMPLIANCE, INCLUDING ACCESSIBLE PATH OF TRAVEL, CURB RETURNS, PARKING STALL(S) AND UNLOADING AREAS, BARRIER FREE AMENITIES AND/OR OTHER APPLICABLE SITE IMPROVEMENTS SHALL CONFORM TO THE AMERICANS WITH DISABILITIES ACT, CALIFORNIA TITLE 24, AND THE CALIFORNIA BUILDING CODE, CURRENT EDITION(S).
 - CONTRACTOR SHALL FIELD VERIFY ALL GRADES AND SLOPES PRIOR TO THE PLACEMENT OF CONCRETE AND/OR PAVEMENT FOR CONFORMANCE WITH ADA ACCESS COMPLIANCE REQUIREMENTS. EXAMPLES OF MINIMUM AND MAXIMUM LIMITS RELATED TO ADA ACCESS COMPLIANCE INCLUDE, BUT ARE NOT LIMITED TO:
 - A) ACCESSIBLE PATH OF TRAVEL CROSS-SLOPE SHALL NOT EXCEED 2.00%
 - B) ACCESSIBLE PATH OF TRAVEL LONGITUDINAL SLOPES SHALL NOT EXCEED 5.00%
 - C) RAMP LONGITUDINAL SLOPES SHALL NOT EXCEED 8.33%
 WALKS SHALL NOT HAVE LESS THAN 48 INCHES IN UNOBSTRUCTED WIDTH
 - CONTRACTOR MUST IMMEDIATELY NOTIFY THE ENGINEER OF RECORD, IDENTIFIED BY THE PROFESSIONAL ENGINEERING SEAL AND SIGNATURE ON THESE PLANS, OF ANY SITE CONDITION(S) AND/OR DESIGN INFORMATION THAT PREVENTS THE CONTRACTOR FROM COMPLYING WITH THE LAWS, REGULATIONS AND/OR BUILDING CODES GOVERNING ADA ACCESS COMPLIANCE.
 - GROUND SLOPES AWAY FROM BUILDING PADS IN LANDSCAPED OR DIRT AREAS SHALL BE NO LESS THAN 5.0% FOR AT LEAST TEN (10) FEET, OR AS OTHERWISE NOTED ON THE PLANS.
 - DRAINAGE SHALL NOT BE ALLOWED ONTO ADJACENT PROPERTY.
6. ALL FILL MATERIAL USED TO SUPPORT THE FOUNDATIONS OF ANY BUILDING OR STRUCTURE SHALL BE PLACED UNDER THE DIRECTION OF A LICENSED GEOTECHNICAL ENGINEER, AND IN COMPLIANCE WITH THE PROJECT SPECIFICATIONS. A SOILS COMPACTION REPORT SHALL BE SUBMITTED TO THE ENGINEER OF RECORD AS REQUIRED BY THE PROJECT SPECIFICATIONS.
7. THE CONTRACTOR SHALL IMPLEMENT DUST CONTROL MEASURES AS REQUIRED BY GOVERNING PUBLIC AGENCIES.
8. THE CONTRACTOR SHALL IMPLEMENT A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) AS REQUIRED BY GOVERNING PUBLIC AGENCIES.
9. CONTRACTOR TO MATCH EXISTING PAVEMENT GRADE AT ALL NEW PAVEMENT LOCATIONS UNLESS OTHERWISE NOTED ON THE PLANS.



PROJECT DATA

TOTAL SITE AREA: 2.62 ACRE
 NORTH STORAGE SPACE AREA: 1.01 ACRE
 SOUTH STORAGE SPACE AREA: 1.19 ACRE
 CUT: 4076.30 CU. YD.
 FILL: 7177.22 CU. YD.
 IMPORT: 3098.93 CU. YD.

PLANS PREPARED BY:

Blair Church & Flynn
 CONSULTING ENGINEERS

485 Clavin Avenue, Suite 200
 Chino, California 91710
 Tel: (909) 338-5400
 Fax: (909) 338-5400

REGISTERED PROFESSIONAL ENGINEER
 No. 78,395
 Paul Alga
 CIVIL
 STATE OF CALIFORNIA
 5/20/2019
 DATE SIGNED

NO.	REVISIONS	DATE	SAP	W0	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.	REVISIONS	DATE	SAP	W0	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	
	BACKCHECK SUBMITTAL	01-30-19					PN	DM		PN												
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LOCATION: SHAVER LAKE, CALIFORNIA

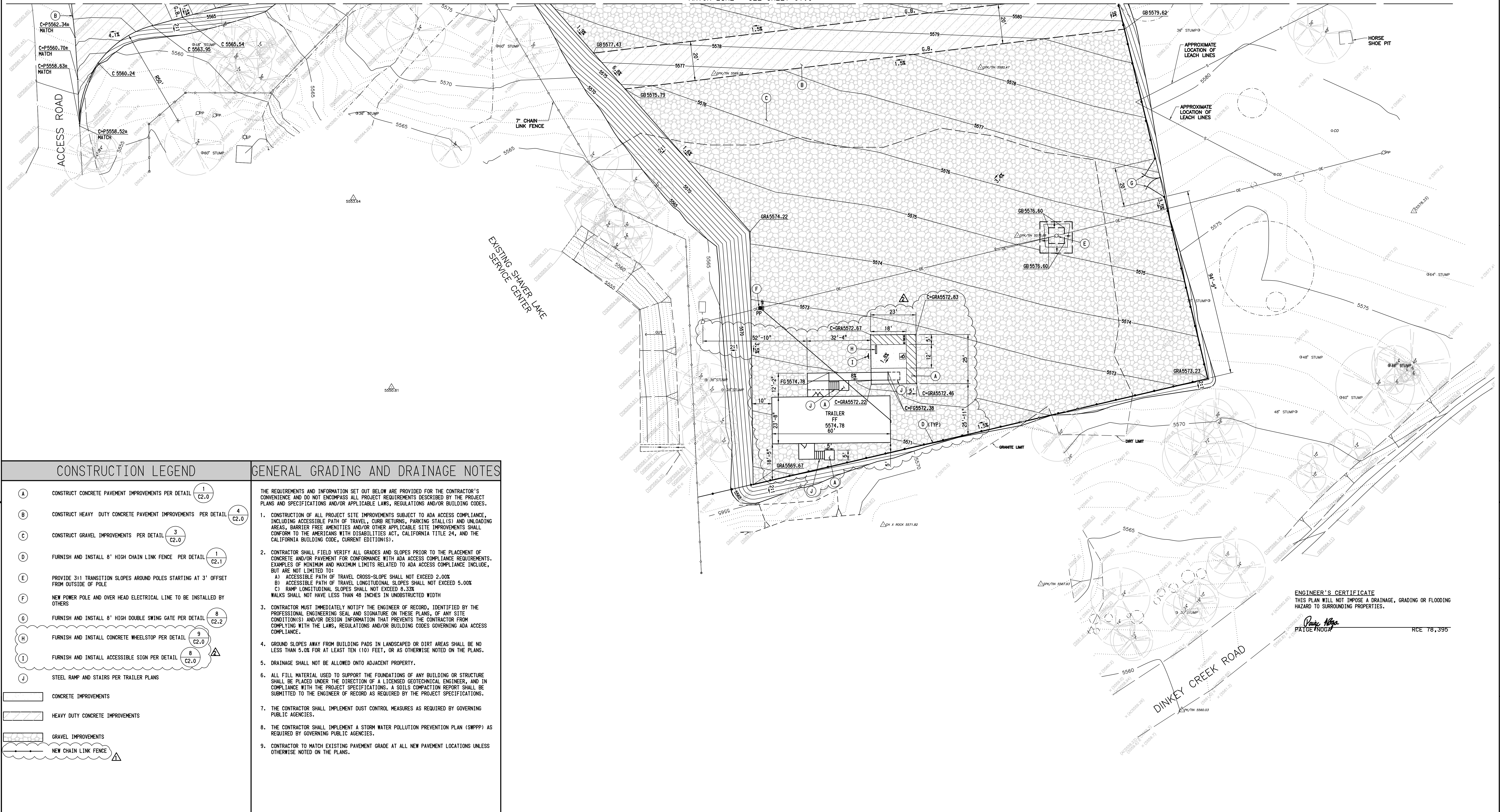
SHAVER LAKE SERVICE CENTER
 LAYDOWN YARD
 SITE AND GRADING PLAN

EDISON An EDISON INTERNATIONAL Company

SHEET NO.
C1.0
 SCALE: 1" = 20'
 5 OF 11 SHTS.

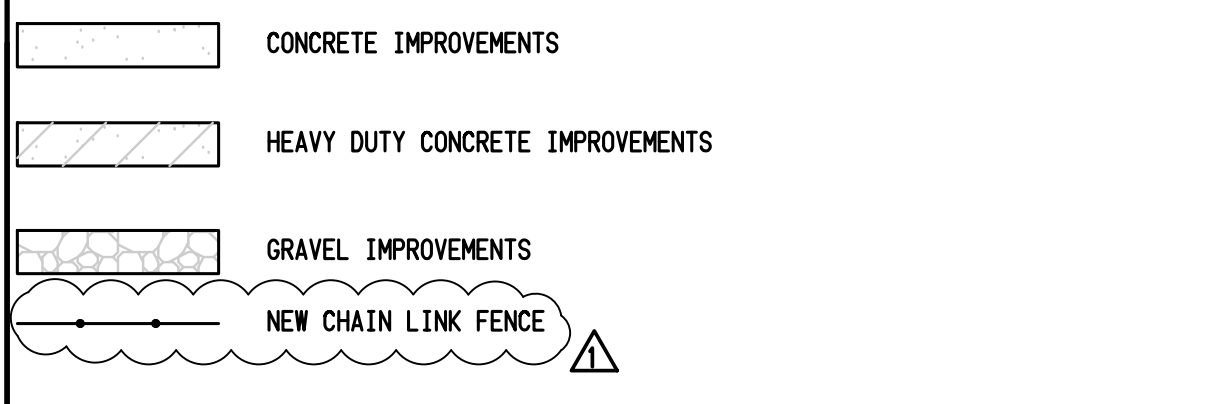
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 SOUTHERN CALIFORNIA EDISON
 2012 - 5 SIZE

MATCH LINE - SEE SHEET C1.0



CONSTRUCTION LEGEND

- (A) CONSTRUCT CONCRETE PAVEMENT IMPROVEMENTS PER DETAIL $\frac{1}{C2.0}$
- (B) CONSTRUCT HEAVY DUTY CONCRETE PAVEMENT IMPROVEMENTS PER DETAIL $\frac{4}{C2.0}$
- (C) CONSTRUCT GRAVEL IMPROVEMENTS PER DETAIL $\frac{3}{C2.0}$
- (D) FURNISH AND INSTALL 8' HIGH CHAIN LINK FENCE PER DETAIL $\frac{1}{C2.1}$
- (E) PROVIDE 3:1 TRANSITION SLOPES AROUND POLES STARTING AT 3' OFFSET FROM OUTSIDE OF POLE
- (F) NEW POWER POLE AND OVER HEAD ELECTRICAL LINE TO BE INSTALLED BY OTHERS
- (G) FURNISH AND INSTALL 8' HIGH DOUBLE SWING GATE PER DETAIL $\frac{8}{C2.2}$
- (H) FURNISH AND INSTALL CONCRETE WHEELSTOP PER DETAIL $\frac{9}{C2.0}$
- (I) FURNISH AND INSTALL ACCESSIBLE SIGN PER DETAIL $\frac{8}{C2.0}$
- (J) STEEL RAMP AND STAIRS PER TRAILER PLANS



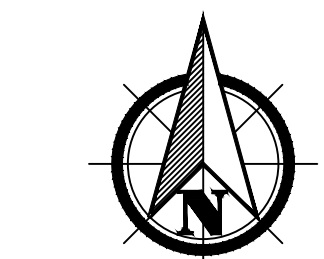
GENERAL GRADING AND DRAINAGE NOTES

1. CONSTRUCTION OF ALL PROJECT SITE IMPROVEMENTS SUBJECT TO ADA ACCESS COMPLIANCE, INCLUDING ACCESSIBLE PATH OF TRAVEL, CURB RETURNS, PARKING STALL(S) AND UNLOADING AREAS, BARRIER FREE AMENITIES AND/OR OTHER APPLICABLE SITE IMPROVEMENTS SHALL CONFORM TO THE AMERICANS WITH DISABILITIES ACT, CALIFORNIA TITLE 24, AND THE CALIFORNIA BUILDING CODE, CURRENT EDITION(S).
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GRADING LEGEND

- C CONCRETE
- FG FINISHED GRADE
- GB GRADE BREAK
- GRA GRAVEL
- P PAVEMENT
- GRA5883.00 NEW FINISHED SURFACE GRADE
- 1.5% DIRECTION OF DRAINAGE
- 5585 MAJOR CONTOURS
- MINOR CONTOURS

ENGINEER'S CERTIFICATE
 THIS PLAN WILL NOT IMPOSE A DRAINAGE, GRADING OR FLOODING HAZARD TO SURROUNDING PROPERTIES.
 Paul Alga
 PAIGE TRUCK
 RCE 78,395



PLANS PREPARED BY:
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 Chula, California 92002
 Tel: (619) 528-6400
 Fax: (619) 528-5900



LOCATION: SHAVER LAKE, CALIFORNIA

SHAVER LAKE SERVICE CENTER
 LAYDOWN YARD
 SITE AND GRADING PLAN

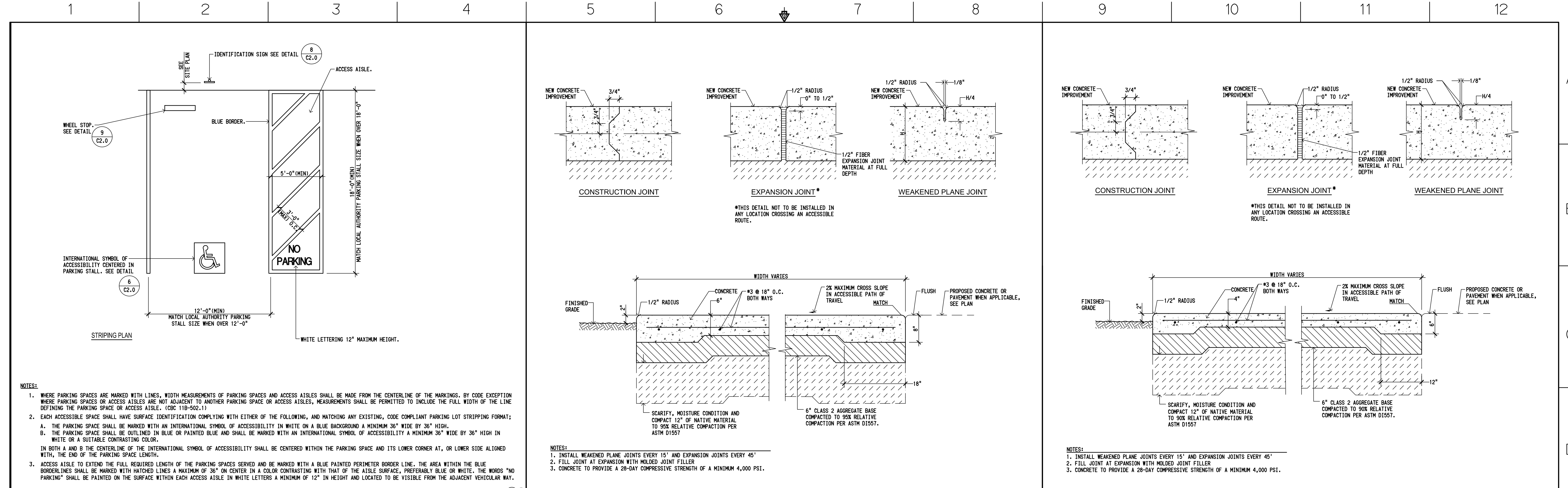
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SCALE: 1" = 20'
 6 OF 11 SHTS.

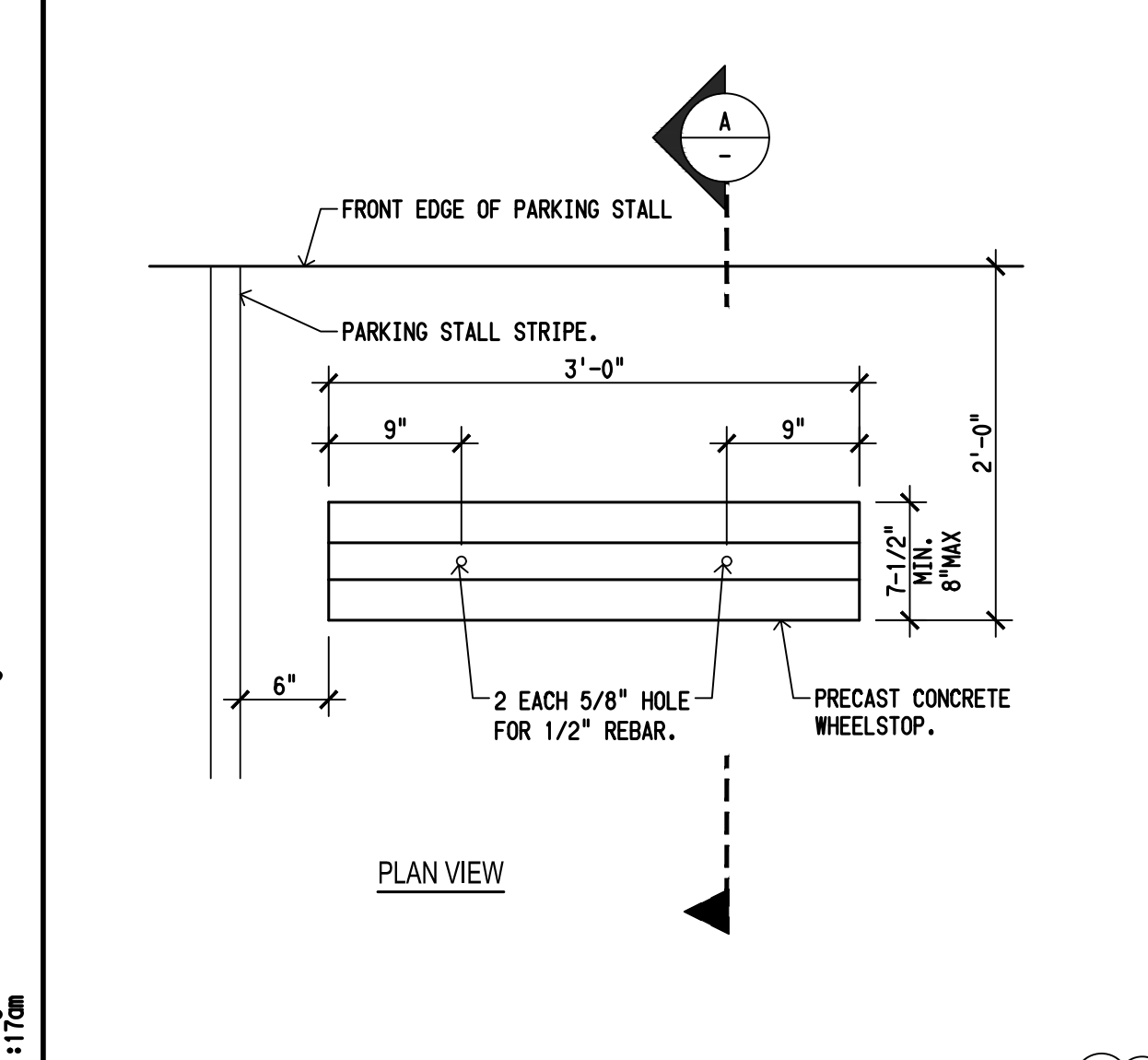
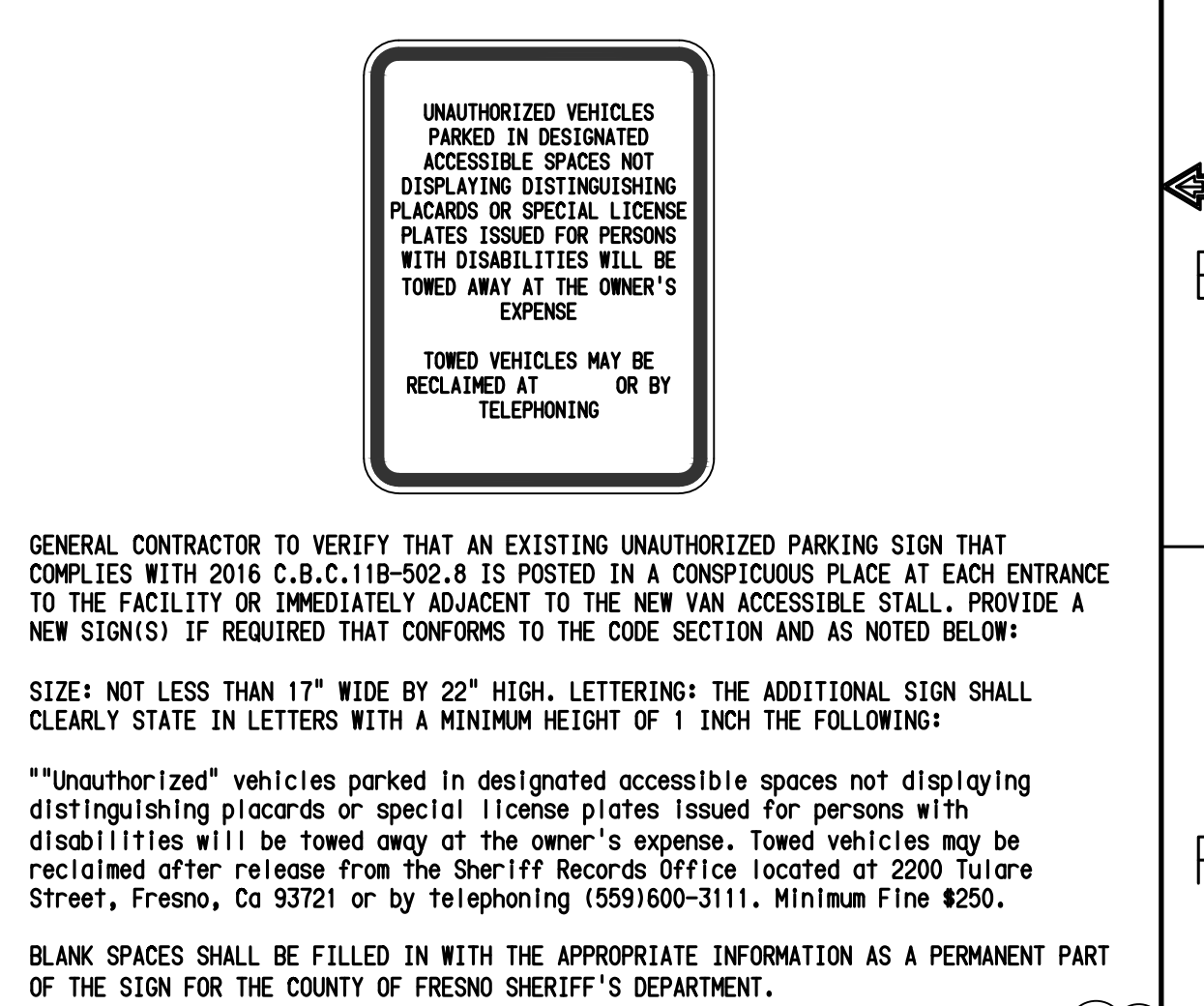
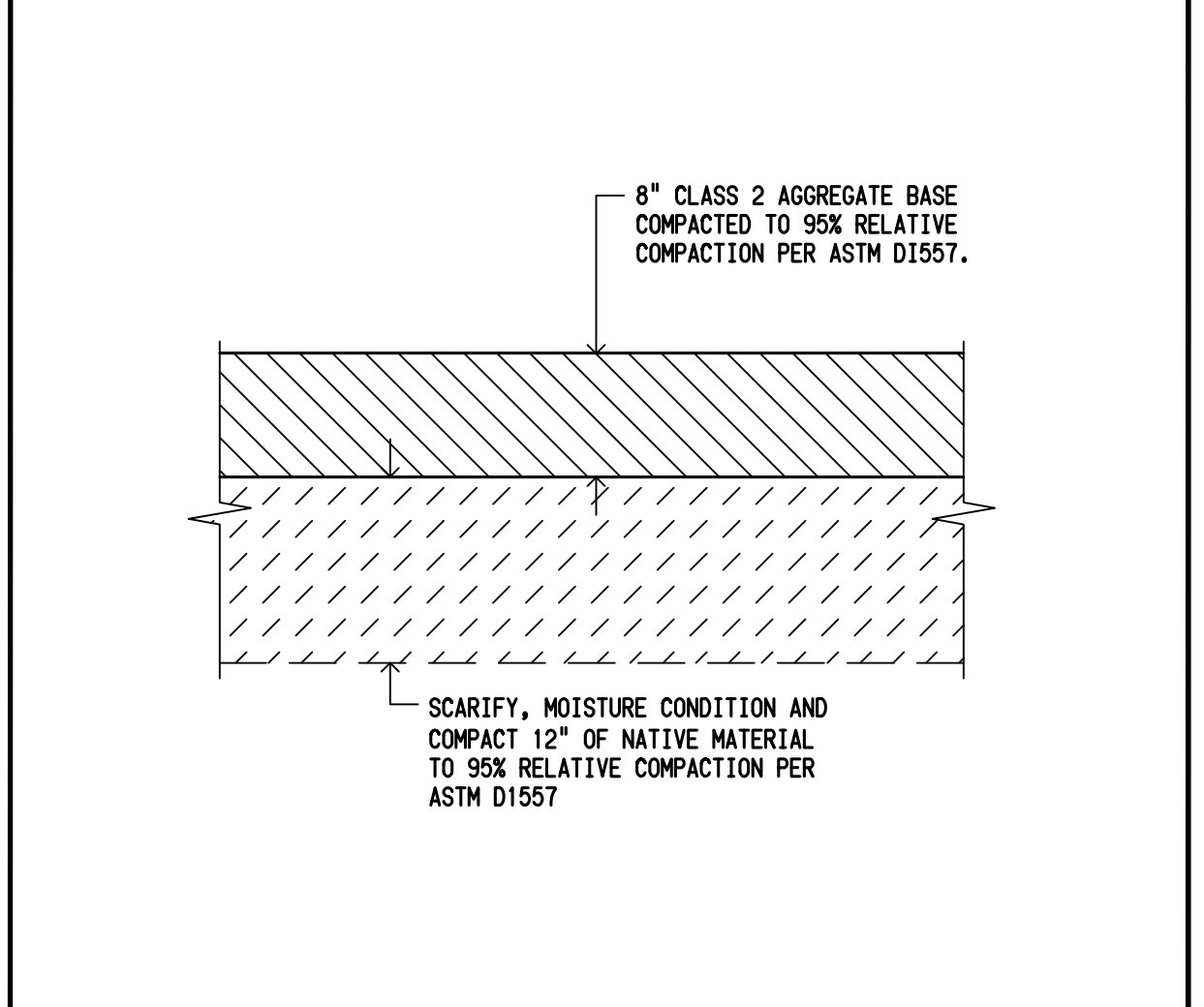
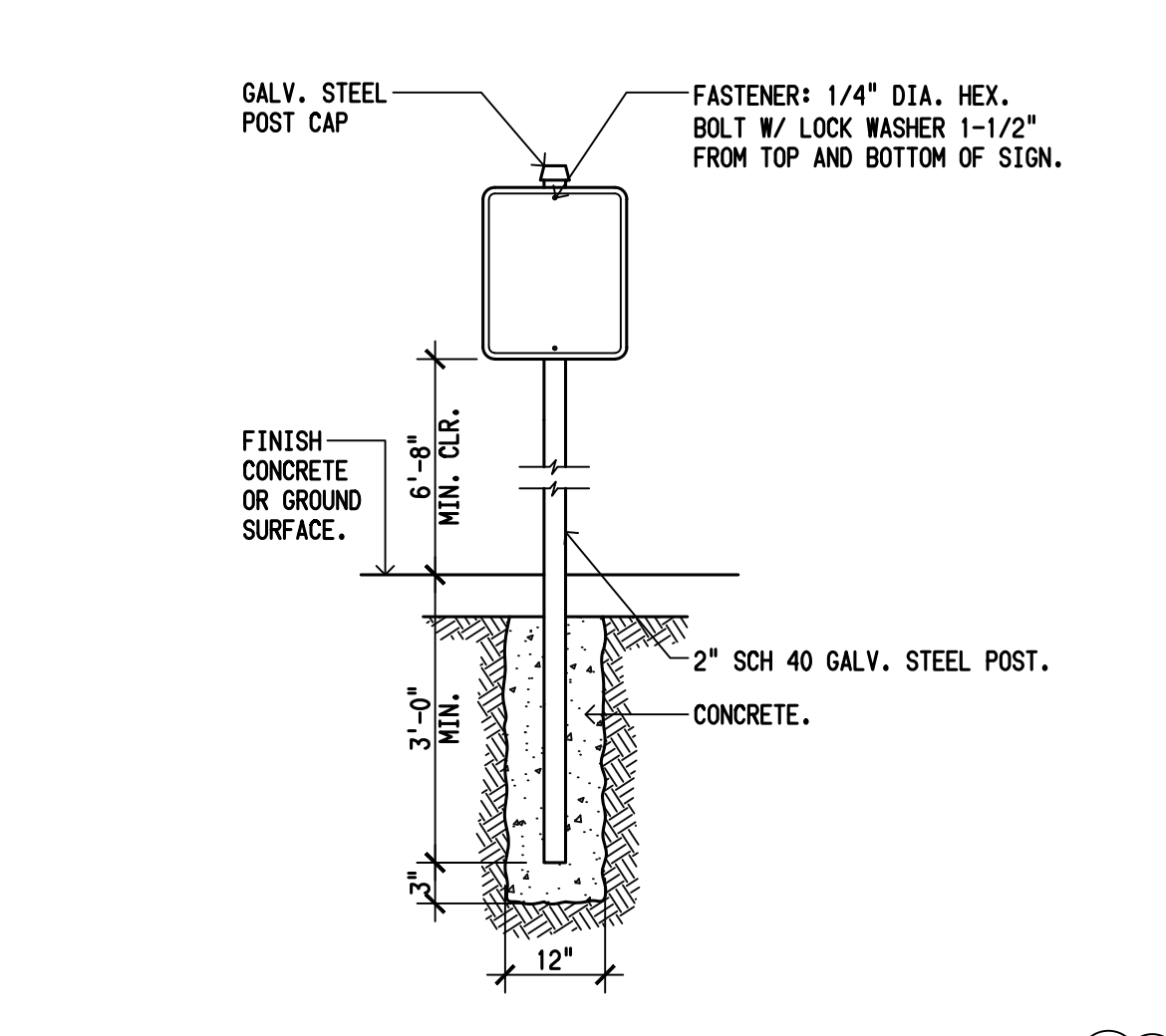
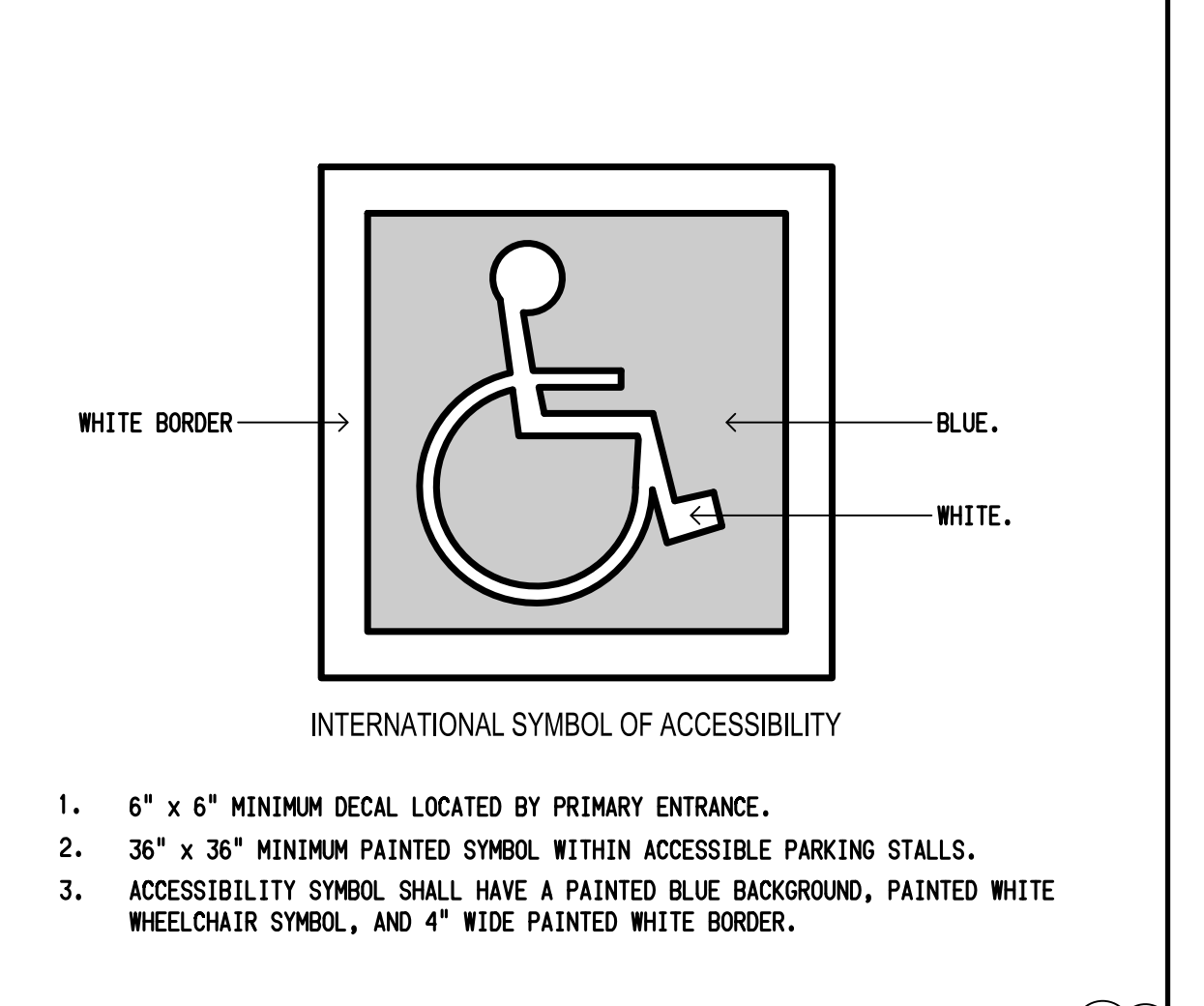
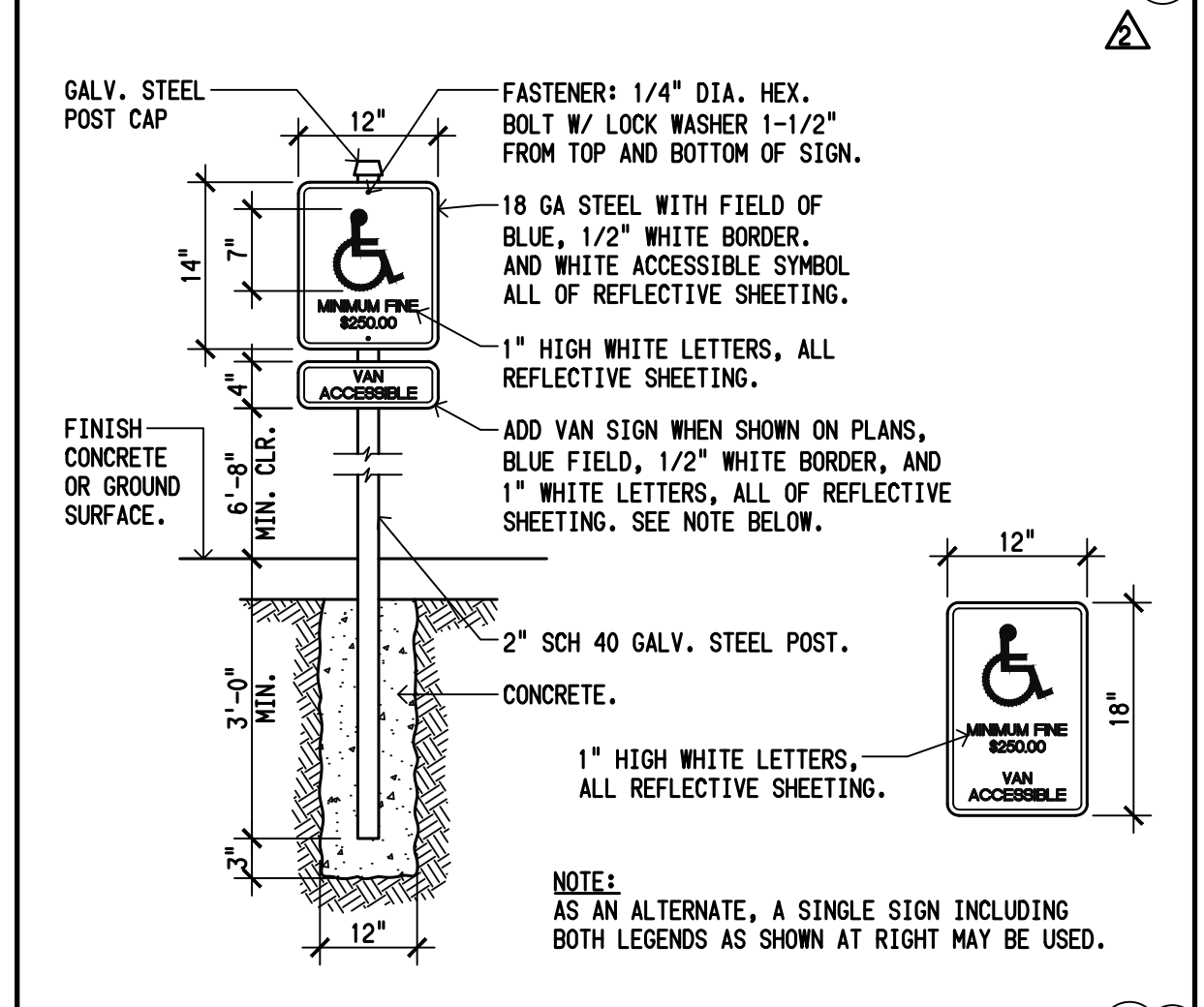
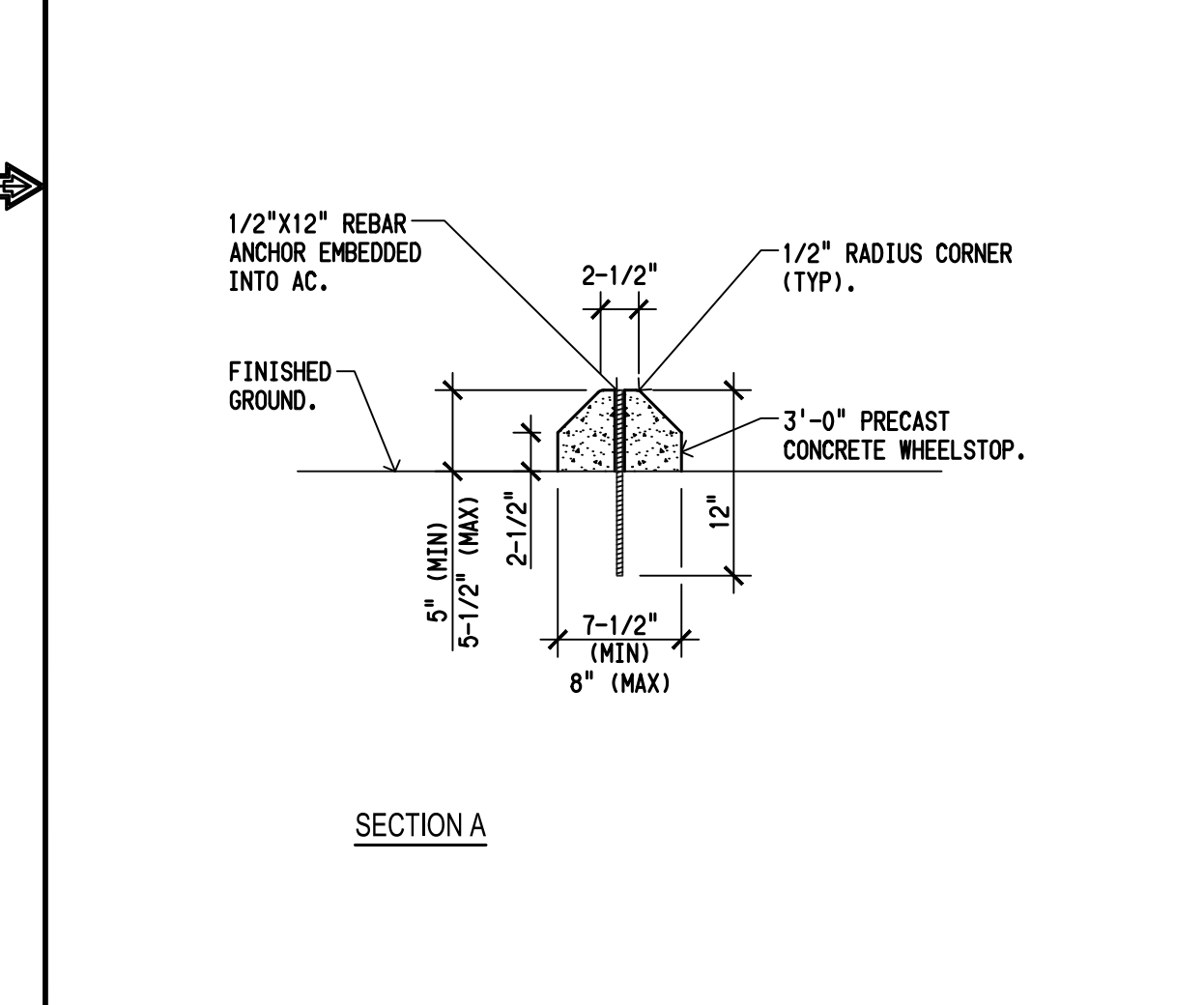
EDISON An EDISON INTERNATIONAL Company

NO.	REVISIONS	DATE	SAP	W0	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.	REVISIONS	DATE	SAP	W0	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	
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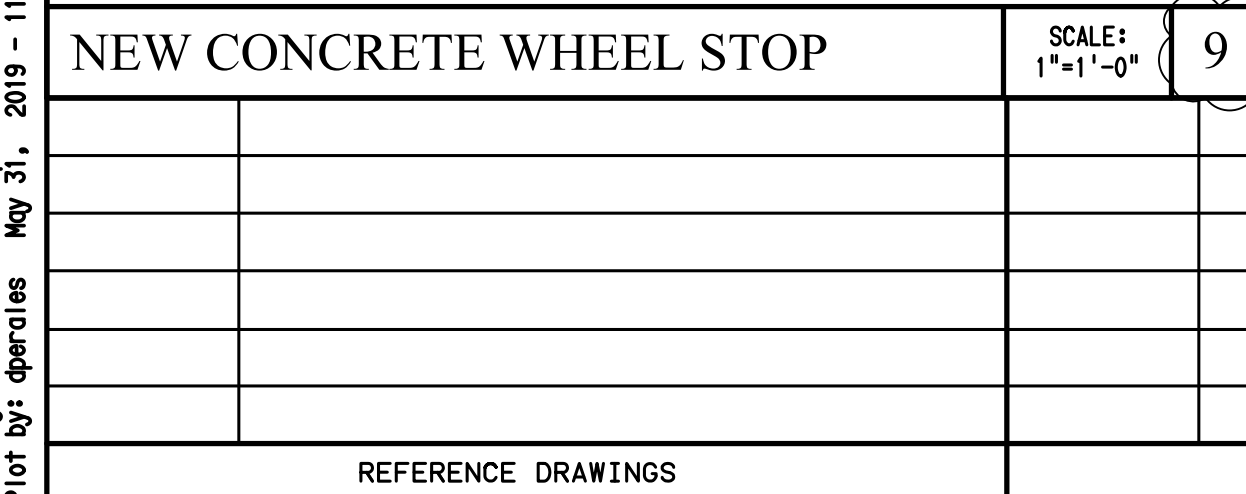
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 2012 - 5 SIZE



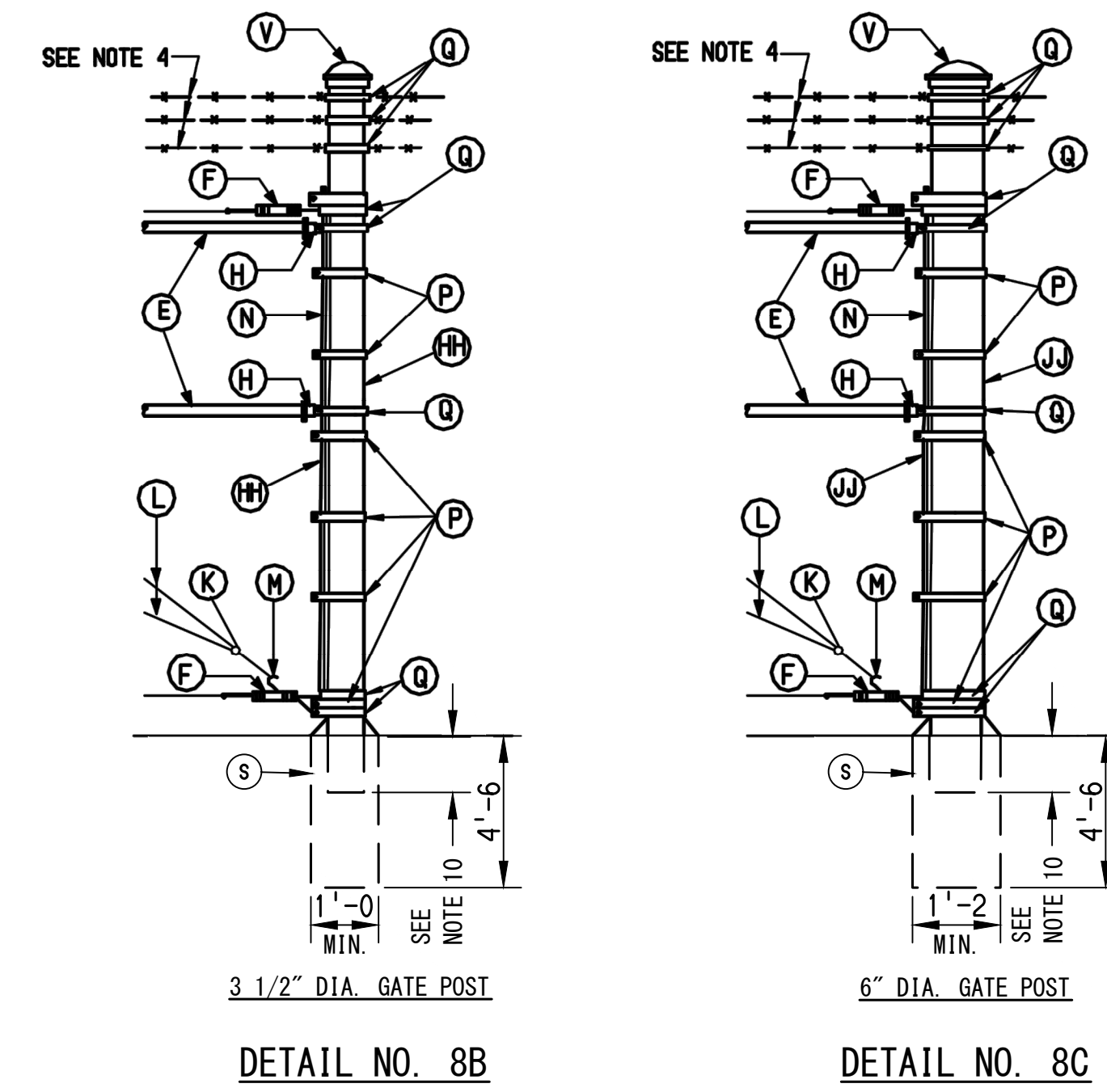
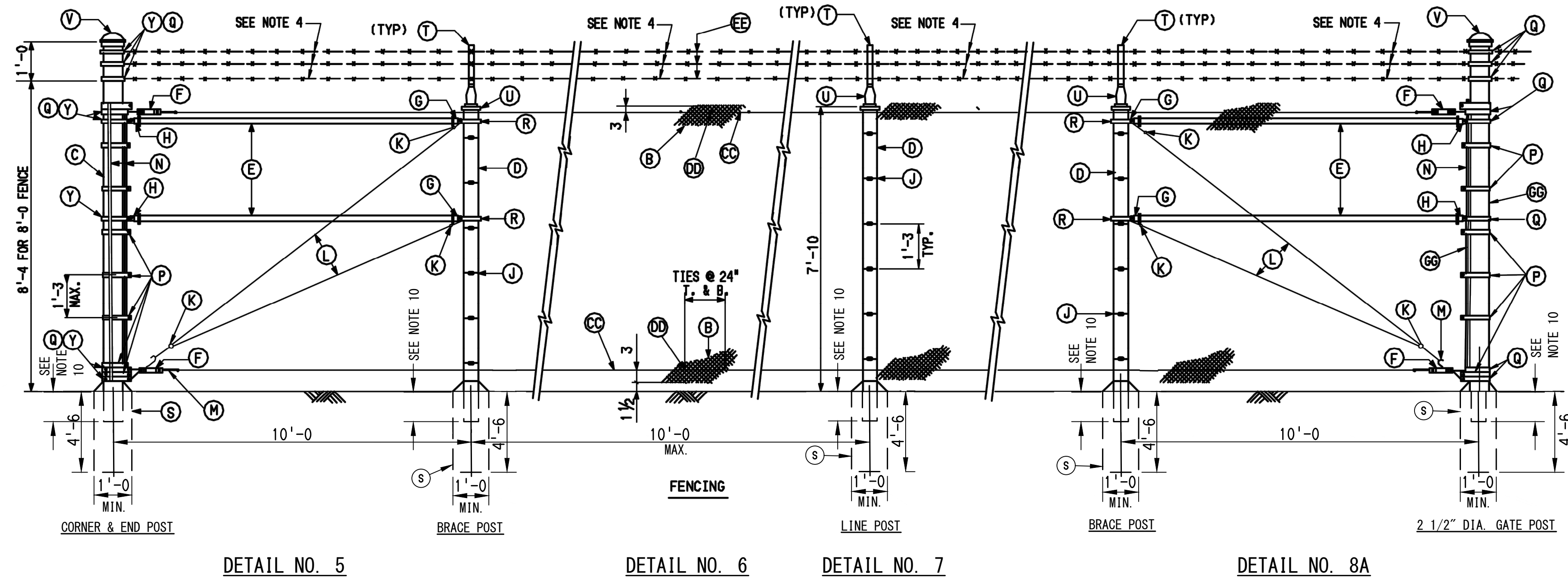
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ACCESSIBLE SIGN SCALE: 3/4"=1'-0" 8 ACCESSIBILITY SYMBOL SCALE: 3/4"=1'-0" 6 SIGN POST SCALE: 3/4"=1'-0" 5 GRAVEL SCALE: 1"=1'-0" 3 UNAUTHORIZED PARKING SIGN SCALE: 1/4"=1'-0" 2

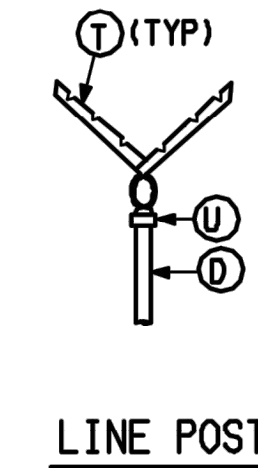
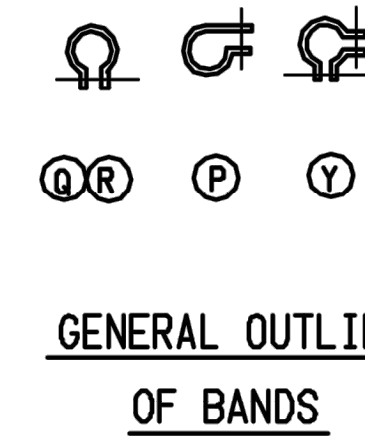
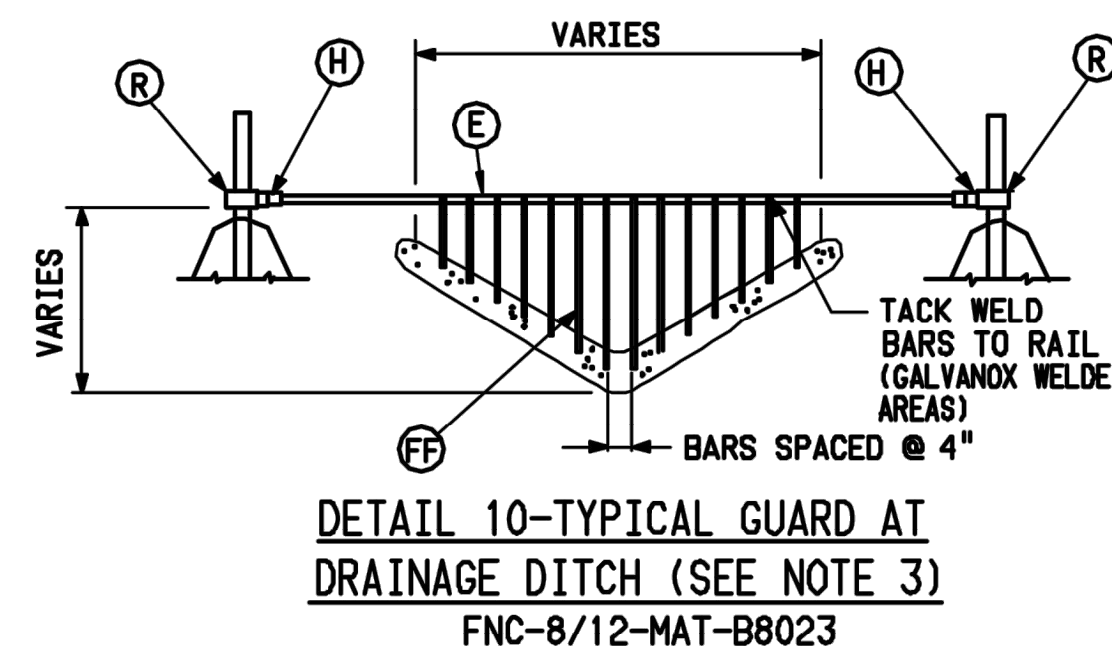
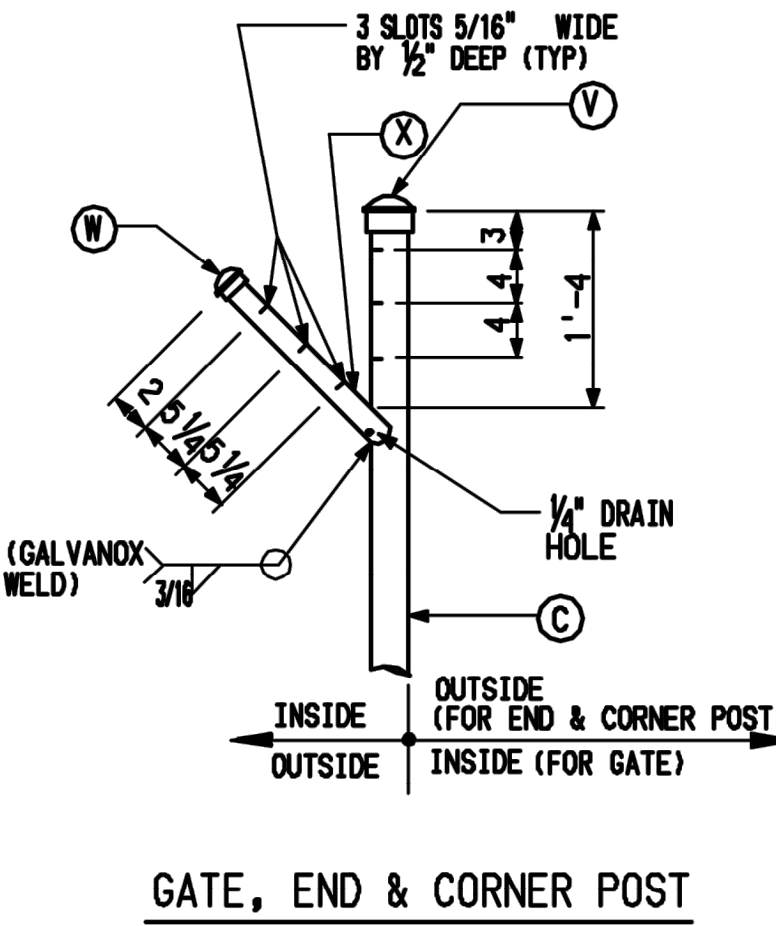
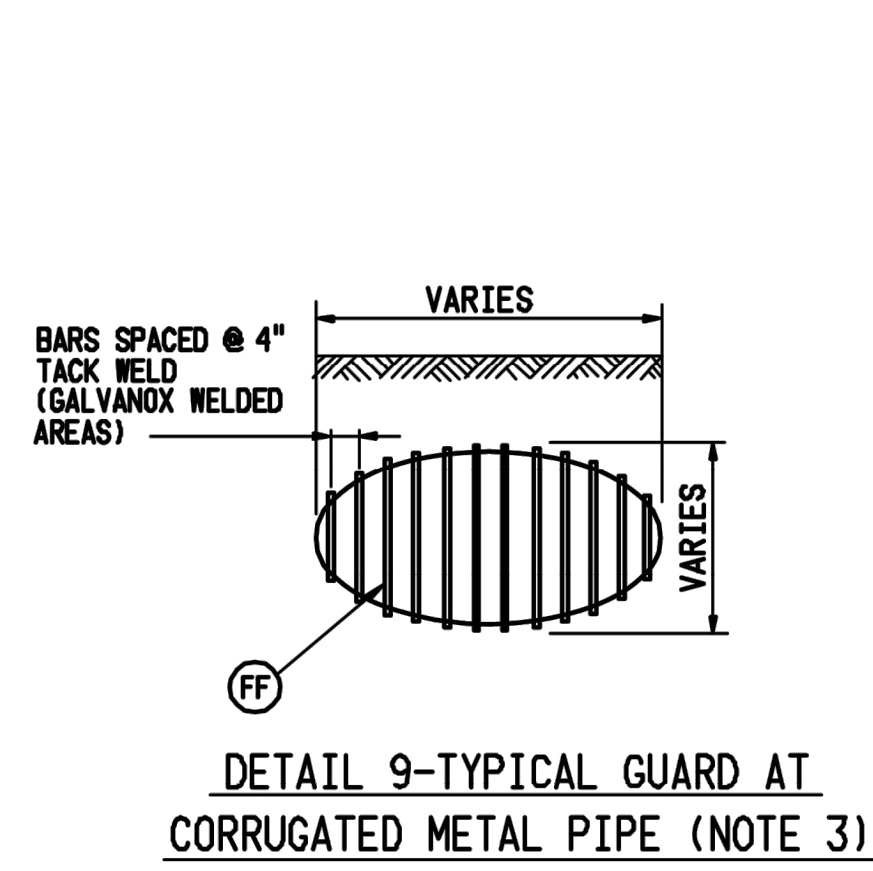


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1	BACKCHECK SUBMITTAL	01-30-19					PN	DM		PN													
2	BACKCHECK SUBMITTAL	05-30-19					PN	DM		PN													



MATERIAL LIST	
ITEM	DESCRIPTION
B	CHAIN LINK FENCE FABRIC, 9 GAUGE GALV. STEEL WIRE, 2" x 2", 8'-0" HIGH
C	CORNER POSTS AND END POSTS 2 1/2" DIA. PIPE FOR 8'-0" FENCE 3 1/2" DIA. PIPE FOR 12'-0" FENCE
D	2" DIA. PIPE FOR 8'-0" FENCE
E	1 1/4" # PIPE, RAIL
F	3/8" x 6" EYE & EYE TURNBUCKLE
G	RAIL END FOR 1 1/4" # PIPE, 1 3/8" # O.D. W/ TENSION EYE
H	RAIL END FOR 1 1/4" # PIPE, 1 3/8" # O.D.
J	9 GA. ALUMINUM TIE WIRE
K	CLIP, 1/4" CROSSBY WIRE ROPE
L	1/4" GALV. STEEL STRAND OR APPROVED EQUAL
M	TRUSS ROD TIGHTENER (1/2" MIN. EYE BOLT W/ HEX NUT 6" MIN. THREAD)
N	3/8" x 3/4" TENSION BAR
P	TENSION BAND (1/8" x 1") FOR 2 1/2" # PIPE TENSION BAND (1/8" x 1") FOR 3 1/2" # PIPE TENSION BAND (1/8" x 1") FOR 6" # PIPE

MATERIAL LIST	
ITEM	DESCRIPTION
Q	BRACE BAND (1/8" x 1") FOR 2 1/2" # PIPE BRACE BAND (1/8" x 1") FOR 3 1/2" # PIPE BRACE BAND (1/8" x 1") FOR 6" # PIPE
R	BRACE BAND (1/8" x 1") FOR 2" # PIPE
S	POST FOUNDATION, CONCRETE 2500 PST @ 28 DAYS POST FOUNDATION CAP MAYBE SUBSTITUTED WITH GROUT
T	EXTENTION ARM FOR MOUNTING BARBED WIRE
U	BARBED WIRE ARM BASE, FOR 2" # PIPE
V	POST CAP FOR 2 1/2" # PIPE POST CAP FOR 3 1/2" # PIPE POST CAP FOR 6" # PIPE
W	POST CAP FOR 1 1/2" # PIPE
X	1 1/2" # PIPE, 1'-6" LONG, GALV.
Y	CORNER BRACE BAND (1/8" x 1") FOR 2 1/2" # PIPE
CC	STIFFENER WIRE, 7 GAUGE GALV. STEEL WIRE
DD	ALUMINUM #4 TIE WIRE, 4" LONG OR HOG RING
EE	BARBED WIRE, 12 1/2 GAUGE GALV. METAL
FF	#5 ASTM A615 GR. 60 BAR
GG	2 1/2" DIA. PIPE, GATE POST
HH	3 1/2" DIA. PIPE, GATE POST
JJ	6" DIA. PIPE, GATE POST



GENERAL NOTES

- ALL MATERIAL SHALL BE HOT DIP GALVANIZED.
- FOR TOP AND BOTTOM TENSION WIRES (CC), PROVIDE ONE TURNBUCKLE FOR EACH RUN OF 300 FEET MAXIMUM.
- AT LOCATIONS ALONG THE FENCE WHERE IT IS IMPOSSIBLE TO MAKE THE FENCE FABRIC CONFORM TO THE IRREGULARITIES OF THE EARTH'S SURFACE, SUCH AS DRAINAGE DITCHES, APPROVED GUARDS SHALL BE PROVIDED TO PREVENT ENTRY FROM UNDER THE FENCE. CORRUGATED METAL PIPING (8" DIA. AND ABOVE) WHICH EXTEND FROM THE INSIDE TO THE OUTSIDE OF SUBSTATION FENCING, SHALL BE EQUIPPED WITH GUARDS ON BOTH PIPE ENDS. SEE GRADING DRAWING FOR TYPICAL GUARD LOCATION.
- 3 STRANDS OF BARBED WIRE OUTSIDE OF SUBSTATION AND 2 STRANDS OF BARBED WIRE INSIDE. SEE SECTION "A-A" ECS 71-03-02, S4 TYPICAL ALL FENCES.
- FOR ASSEMBLY CODES SEE ECS 71-03-01, S1.
- ALL PIPE SIZES ARE NOMINAL. SEE TABLE 1 ON ECS 71-03-01, S1 FOR ACTUAL O.D.
- ALL PIPE MAY BE SCHEDULE 40 OR TUF-40.
- SEE ECS 71-03-02, S5 FOR GATE POST FOR ROLLING GATES.
- FENCE AND POST SHALL BE COATED GREEN TO MATCH EXISTING.
- FENCE POST EMBEDMENT AS REQUIRED PER FENCE MANUFACTURER.

8 FT. HIGH CHAIN LINK FENCE WITH BARBED WIRE

SCALE: 1/4"=1'-0" 1

PLANS PREPARED BY:

Blair, Church & Flynn
CONSULTING ENGINEERS

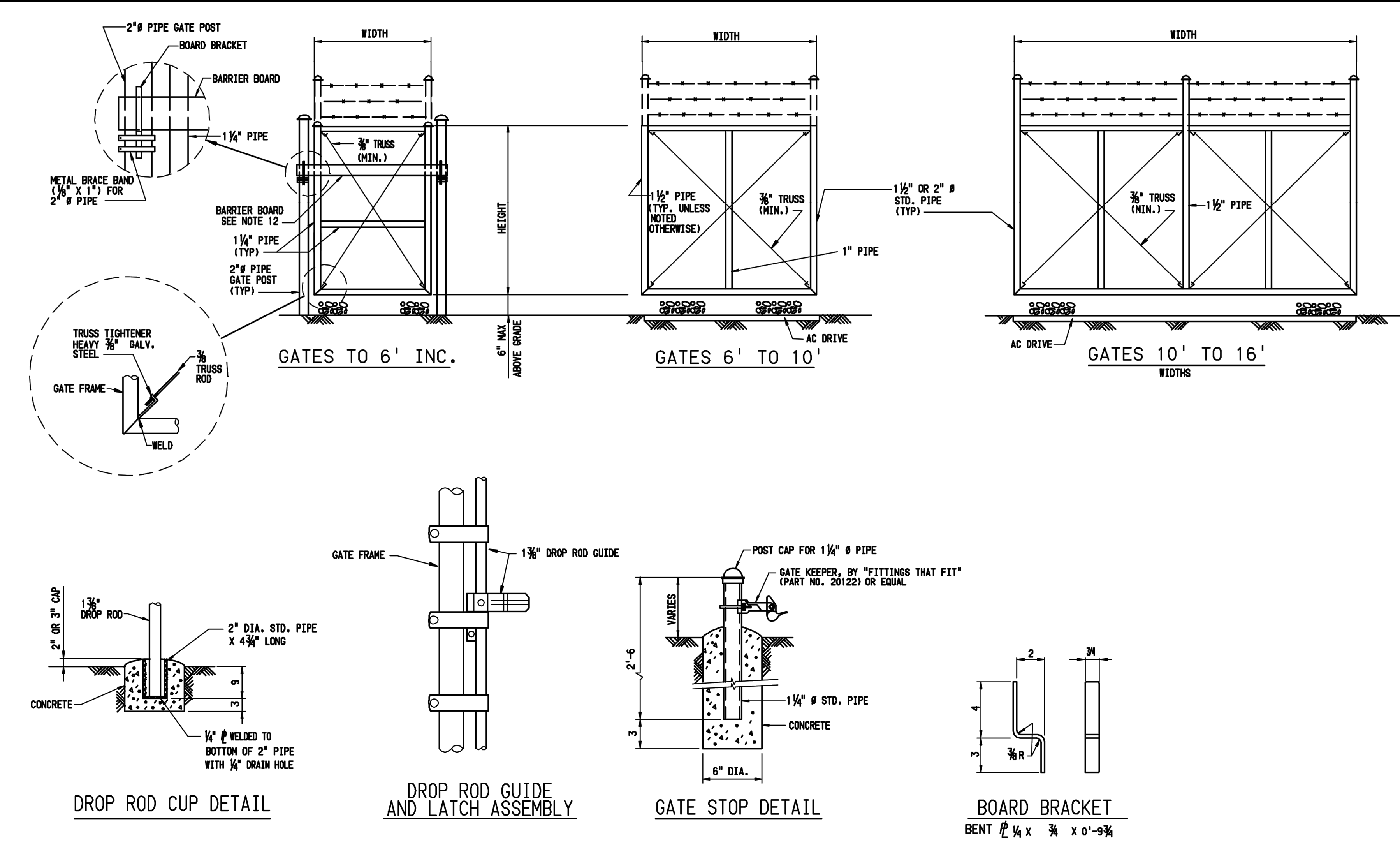
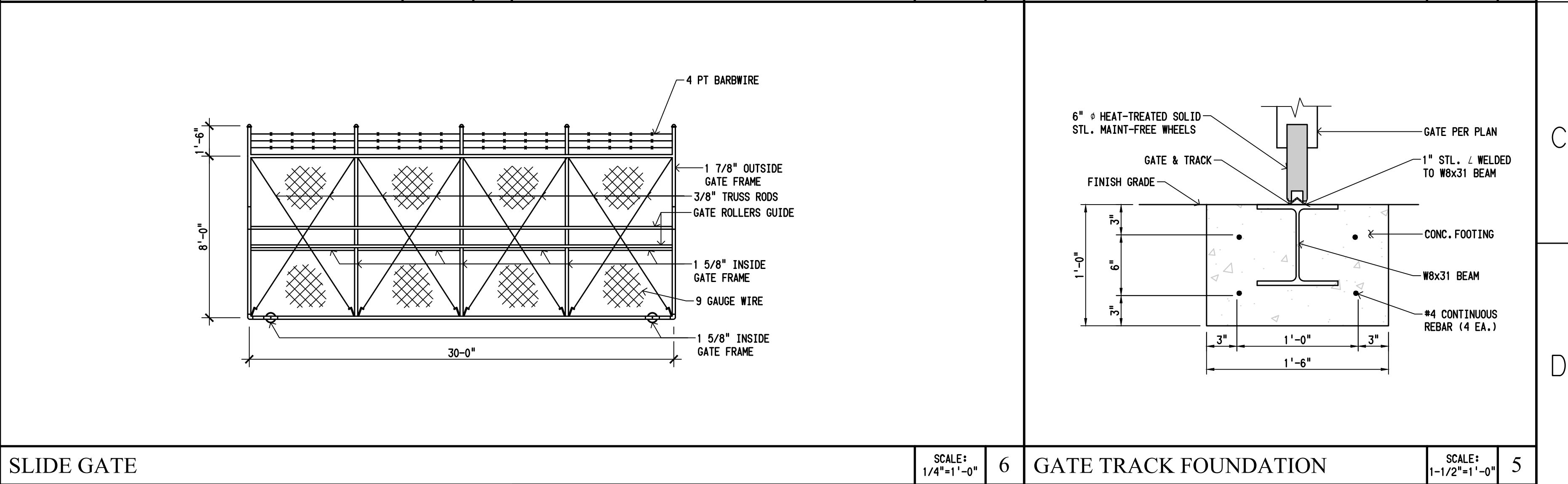
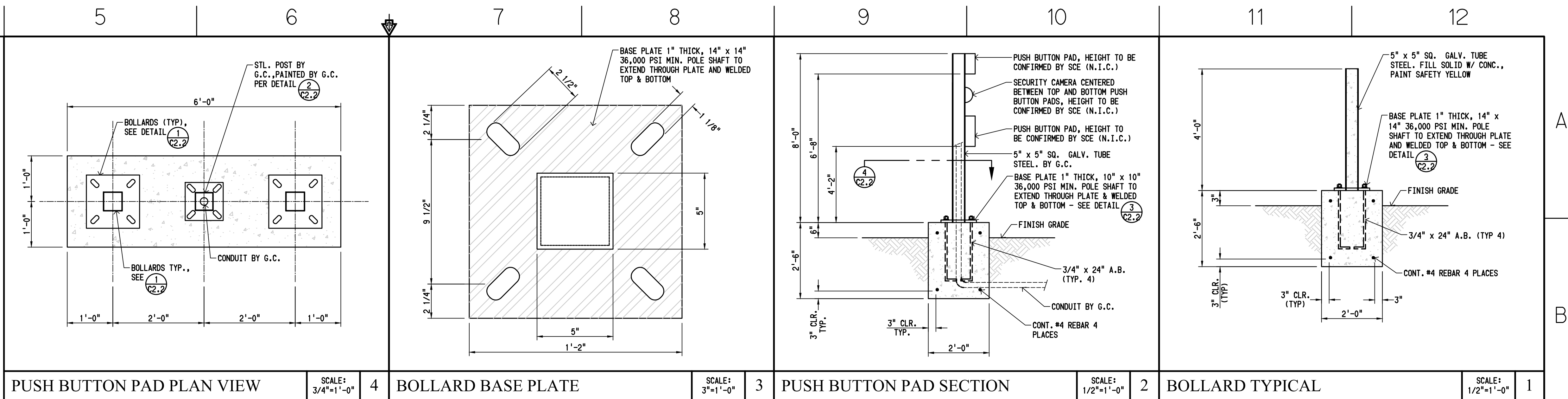
465 Clarks Avenue, Suite 200
Clarks, California 95022
Tel: (925) 524-4200
Fax: (925) 524-2100



LOCATION: SHAVER LAKE, CALIFORNIA
SHAVER LAKE SERVICE CENTER
LAYDOWN YARD
SITE DETAILS

SHEET NO. C2.1
SCALE: AS NOTED
8 OF 11 SHTS.

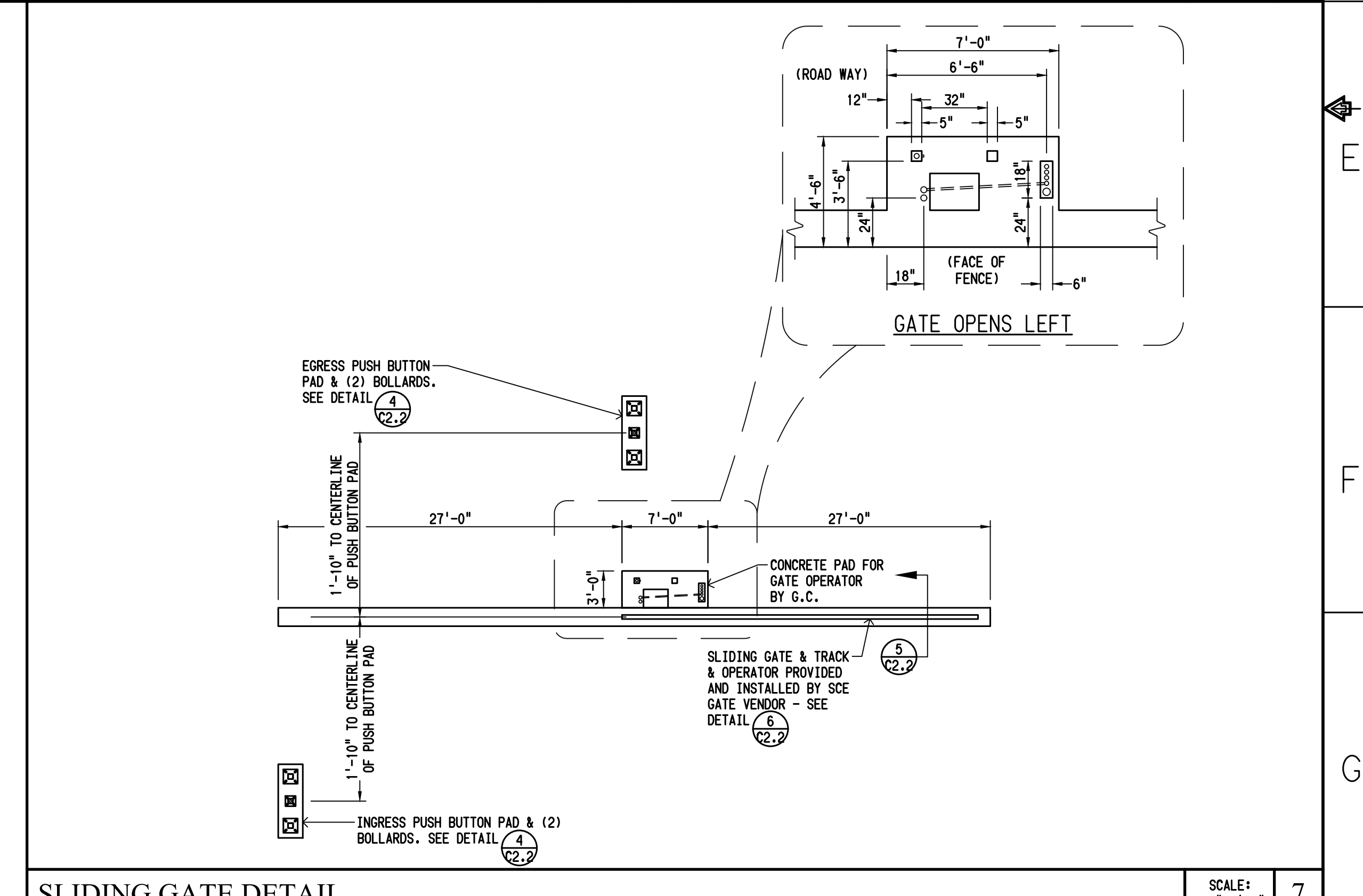
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		BACKCHECK SUBMITTAL	05-30-19			PN	DM	PN																																										



GENERAL NOTES

- THE GATE WIDTH, EITHER SINGLE OR DOUBLE, SHALL BE DETERMINED BY THE FENCE OPENING. THE FENCE OPENING IS THE DISTANCE BETWEEN THE INSIDE FACE OF THE GATE POSTS.
- THE TOP OF THE GATE IS THE SAME AS THE TOP OF THE FENCE FABRIC.
- GATE STOP TO BE BY "FITTINGS THAT FIT" OR EQUAL. GATE HOLDBACKS ASSEMBLY- MALLEABLE, DUCKBILL TYPE.
- 2" PIPE WITH CAME STOP TO BE INSTALLED WHEN LATCHING DEVICE IS USED.
- PROVIDE SLIP SLEEVE ASSEMBLY IN COMBINATION WITH LATCHING DEVICE.
- GATES IN FENCES WHICH HAVE BARBED WIRE EXTENSION SHOULD BE ORDERED WITH STRANDS OF BARBED WIRE. GATES SHOULD ALSO BE ORDERED COMPLETE WITH HINGES, LATCH & STOPS.
- GATE POSTS FOR 4', 6', & 8' FENCES SHALL BE AS FOLLOWS:

GATE POSTS	SINGLE GATE WIDTH	DOUBLE GATE WIDTH	SLIDING GATE WIDTH
2"	TO 4 FT INCL	TO 8 FT INCL	
2 1/2"	OVER 4 FT TO 6 FT	OVER 8 FT TO 12 FT	TO 12 FT INCL
3 1/2"	OVER 6 FT TO 13 FT	OVER 12 FT TO 26 FT	12 FT TO 26 FT
6"	OVER 13 FT TO 18 FT	OVER 26 FT TO 36 FT	OVER 26 FT
- ALL MATERIAL IS TO BE HOT DIP GALVANIZED.
- ALL FITTINGS (INCLUDING POST AND GATE HINGES) WHICH HAVE THREAD AND NUT PARTS EXPOSED SHALL BE WELDED TO THE GATE FRAME ON THE NUT END TO PREVENT REMOVAL BY THE USE OF HAND TOOLS.
- FOR ROLLING GATES, REFER TO ECS 33-73-28, SH.1 FOR POSITION OF LINE POST WHICH WILL BE PLACED ADJACENT TO ROLLING GATE. THE CORRECT POSITIONING OF THIS LINE POST IS NECESSARY TO ACCOMMODATE ENGAGEMENT OF GROUNDING CLIPS.
- FOR LAYOUT OF FENCE, TOTAL LENGTH, NUMBER & LOCATION OF GATES, SEE FOUNDATION PLAN.
- BARRIER BOARD TO BE MOUNTED ON GATE POST FOR 4 FOOT GATES AT CAPACITOR, OPERATING AND TRANSFER BUS CHAIN LINK FENCE ENCLOSURES.
- ALL PIPE SIZES ARE NOMINAL. SEE TABLE 1 ON ECS 71-03-01, S1 FOR ACTUAL O.D.
- ALL PIPE MAY BE SCHEDULE 40 OR TUF-40.



Drawing: P:\218-0000\Drawings\Automatic Gate Details.dwg: C2.2 Automatic and Swing Gate Details - BCF.ctb
 Plot by: gregorys Date: 3/1/2019 11:11:00am

NO.	REVISIONS	DATE	SAP	W/O	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.	REVISIONS	DATE	SAP	W/O	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	
	BACKCHECK SUBMITTAL	01-30-19					PN	DM	PN													
	BACKCHECK SUBMITTAL	05-30-19					PN	DM	PN													

PLANS PREPARED BY:

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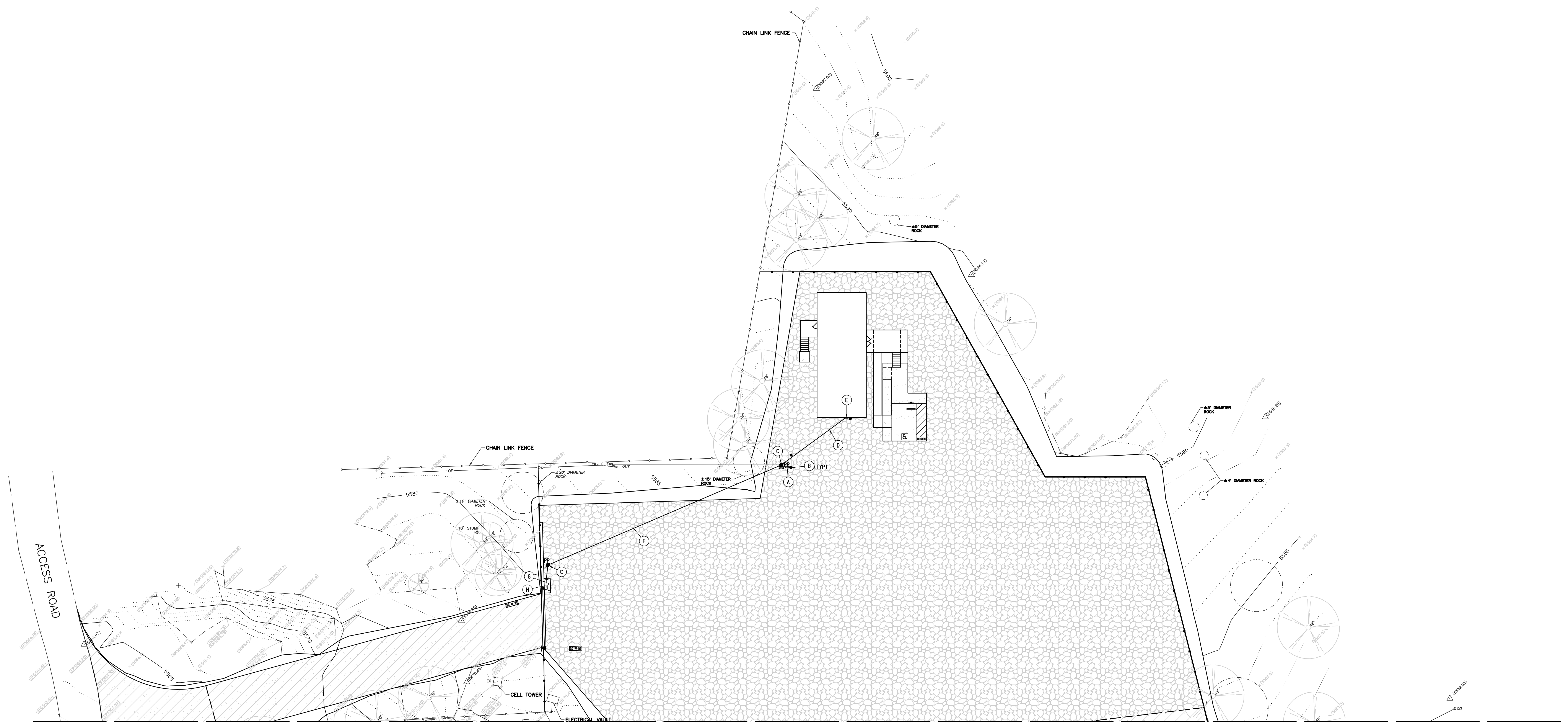
REGISTERED PROFESSIONAL ENGINEER
STATE OF CALIFORNIA
No. 78,395
Date: 6/20/2019
Scale: 1/2"=1'-0"

LOCATION: SHAVER LAKE, CALIFORNIA

SHAVER LAKE SERVICE CENTER
LAYDOWN YARD
AUTOMATIC AND SWING GATE DETAILS

SHEET NO. C2.2

SCALE: 9 OF 11 SHTS.



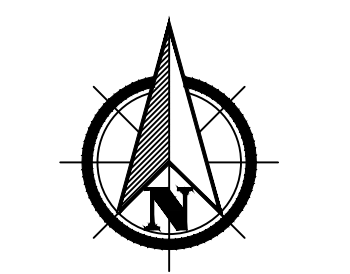
MATCH LINE - SEE SHEET E1.1

ELECTRICAL LEGEND

- (A) FURNISH AND INSTALL 120/240V, 400 AMP SERVICE CT METER AND DISTRIBUTION PANEL "A" PER SCE ESR 2-32 & 2-33 STANDARDS. SEE SINGLE LINE FOR BREAKERS
- (B) GROUND ROD 12" (MIN) BELOW SURFACE
- (C) SCE TO FURNISH AND INSTALL POWER POLE AND POLE HARDWARE (200 AMP PANEL)
- (D) FURNISH AND INSTALL OVERHEAD 1/0 TRIPLEX ACSR FROM SERVICE METER TO FUTURE TRAILERS PER SCE STANDARDS
- (E) 100A, 120/240V, SINGLE PHASE TRAILER PANEL "A". SCE TO FURNISH AND INSTALL EQUIPMENT FOR OVERHEAD SERVICE ENTRANCE
- (F) FURNISH AND INSTALL OVERHEAD #6 TRIPLEX ACSR FROM SERVICE METER TO PROPOSED GATE MOTOR POWER POLE
- (G) TRANSITION FROM #6 TRIPLEX ACSR OVERHEAD TO (1) 1" U.G. SCH. 40 PVC WITH (2) #8 CU THIN-2 AND (1) #10 GND. CONDUIT AND CONDUCTOR INSTALLATION PER SCE STANDARDS
- (H) GATE CONTROLLER AND CHAMBERLAIN ELITE SL3000 1/2 HP MOTOR TO BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS
- (I) 200A, 120/240V, SINGLE PHASE TRAILER PANEL "D". SCE TO FURNISH AND INSTALL EQUIPMENT FOR OVERHEAD SERVICE ENTRANCE
- (J) FURNISH AND INSTALL 120/240V, 400 AMP SERVICE CT METER AND DISTRIBUTION PANEL "B" PER SCE ESR 2-32 & 2-33 STANDARDS. SEE SINGLE LINE FOR BREAKERS

GENERAL NOTES

1. ELECTRICAL UTILITY LINE TO BE PROTECTED IN PLACE WHEN POSSIBLE. IF ANY CONSTRUCTION CONSTRAINTS ARE ENCOUNTERED DURING THE INSTALLATION OF THE ELECTRICAL EQUIPMENT REMOVE AND RELOCATE THE ELECTRICAL LINE AS NEEDED. IF THE ELECTRICAL LINE CANNOT BE SALVAGED THE CONTRACTOR MUST LAWFULLY DISPOSE OF THE ELECTRICAL LINE AND REPLACE WITH LIKE AND KIND.
2. ALL ELECTRICAL POWER IN CLOSE PROXIMITY TO THE INSTALLATION OF THE ELECTRICAL EQUIPMENT MUST BE POWERED OFF PRIOR TO THE START OF CONSTRUCTION, TO PREVENT ANY ELECTRICAL INJURIES.
3. HAND DIG ALL UTILITIES IN CLOSE PROXIMITY TO THE INSTALLATION OF THE ELECTRICAL EQUIPMENT TO AVOID DAMAGING ANY UTILITY LINE.
4. SEE SINGLE LINE DIAGRAM ON SHEET E1.1.
5. THE METHODS CONTAINED IN CEC/NEC ARTICLE 250 SHALL BE FOLLOWED TO COMPLY WITH GROUNDING AND BONDING OF ELECTRICAL SYSTEMS AND NON-CURRENT CARRYING CONDUCTIVE MATERIALS, ENCLOSURES, OR ITEMS FORMING PART OF ANY SUCH EQUIPMENT THAT ENCLOSES OR CARRIES ELECTRICAL CONDUCTOR OR EQUIPMENT THAT IS LIKELY TO BECOME ENERGIZED. SEE CEC/NEC 250.4(A)(1) THROUGH (5) FOR FURTHER DESCRIPTION.
6. WHERE TWO OR MORE GROUND RODS ARE TO BE INSTALLED, THE MINIMUM SEPARATION SHALL BE 6' PER CEC/NEC 250.53 (A)(2), AND (3) RESISTANCE OF ELECTRODES.
7. ALL CONDUITS SHALL COMPLY WITH CEC/NEC TABLE 300.5 MINIMUM BURIAL DEPTH.
8. PER CEC/NEC 110.26 "ACCESS AND WORKING SPACE SHALL BE PROVIDED AND MAINTAINED ABOUT ALL ELECTRICAL EQUIPMENT TO PERMIT READY AND SAFE OPERATION AND MAINTENANCE OF SUCH EQUIPMENT."



PLANS PREPARED BY:
Blair, Church & Flynn
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Fax: (559) 528-4300



Drawing: P:\218-0000\1\product\10000\118000_0102.dwg; E1.0 Partial Electrical Site Plan - BCF.ctb
User: jrg; Operator: jrg; Plot: 3/1/2019 11:11:00am

REFERENCE DRAWINGS	REFERENCE DRAWINGS	NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	NO.	REVISIONS	DATE	SAP	WO	SUPV	APPROVED	ENGR	CK'D	MADE	P.E.	
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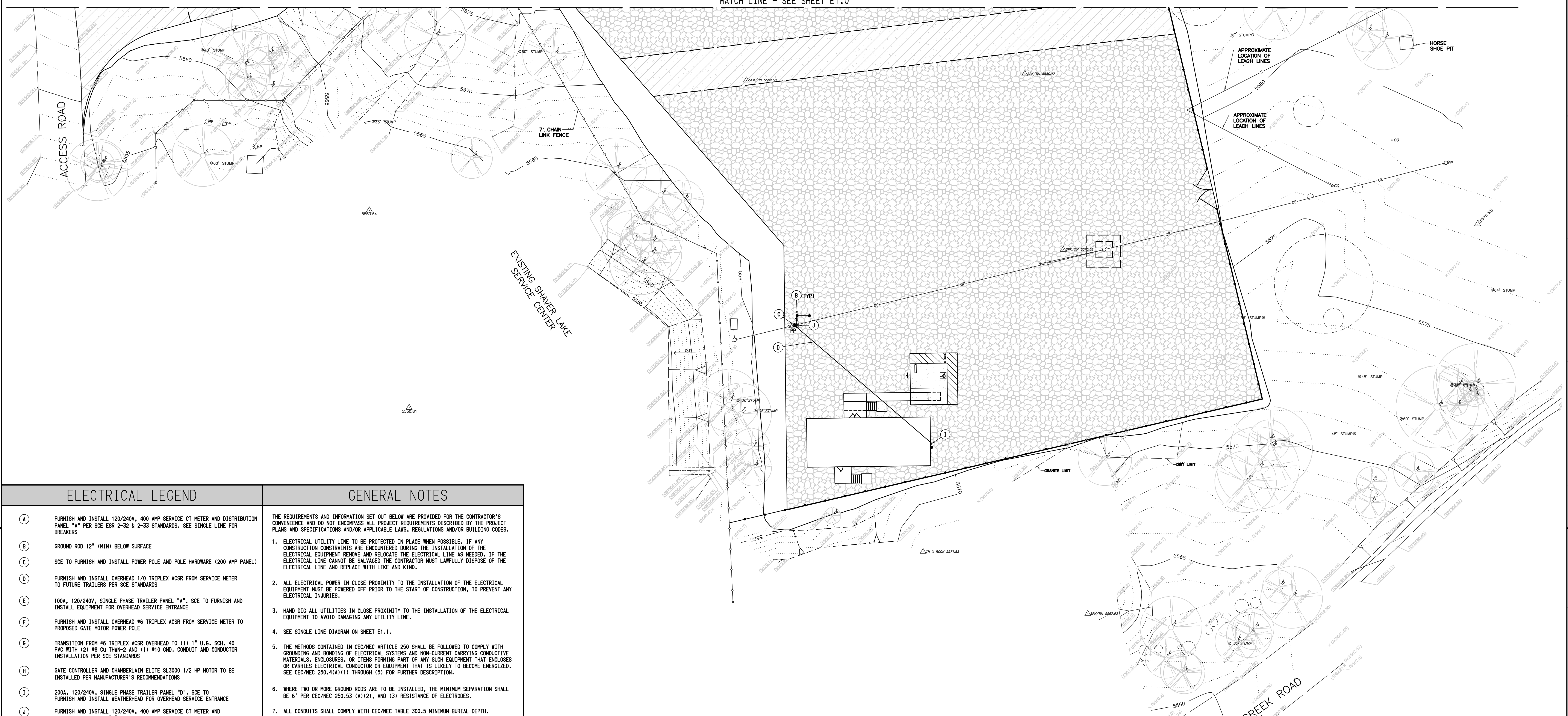
LOCATION: SHAVER LAKE, CALIFORNIA

SHAVER LAKE SERVICE CENTER
LAYDOWN YARD
PARTIAL ELECTRICAL SITE PLAN

EDISON An EDISON INTERNATIONAL Company

SHEET NO. **E1.0**
SCALE: 1" = 20'
19 OF 11 SHTS.

MATCH LINE - SEE SHEET E1.0



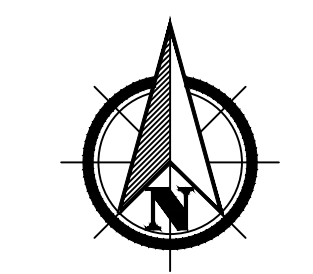
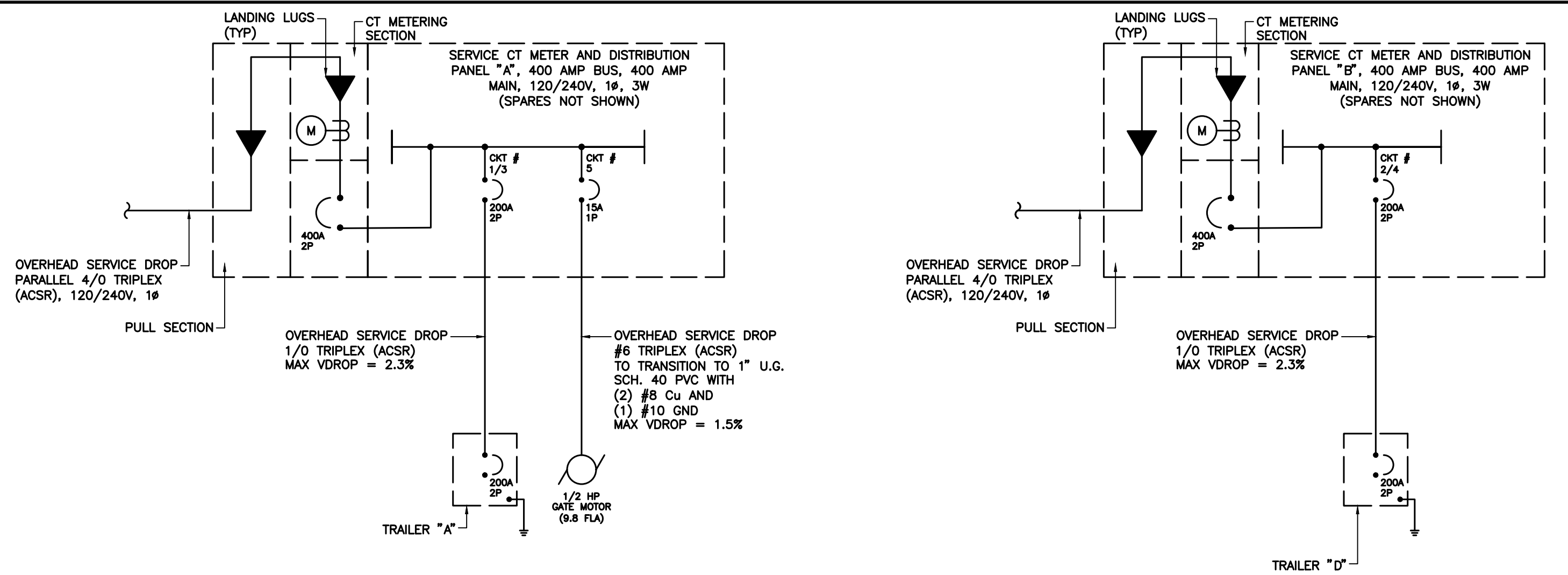
ELECTRICAL LEGEND

- (A) FURNISH AND INSTALL 120/240V, 400 AMP SERVICE CT METER AND DISTRIBUTION PANEL "A" PER SCE ESR 2-32 & 2-33 STANDARDS. SEE SINGLE LINE FOR BREAKERS
 - (B) GROUND ROD 12" (MIN) BELOW SURFACE
 - (C) SCE TO FURNISH AND INSTALL POWER POLE AND POLE HARDWARE (200 AMP PANEL)
 - (D) FURNISH AND INSTALL OVERHEAD 1/0 TRIPLEX ACSR FROM SERVICE METER TO FUTURE TRAILERS PER SCE STANDARDS
 - (E) 100A, 120/240V, SINGLE PHASE TRAILER PANEL "A". SCE TO FURNISH AND INSTALL EQUIPMENT FOR OVERHEAD SERVICE ENTRANCE
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 - (G) TRANSITION FROM #6 TRIPLEX ACSR OVERHEAD TO (1) 1" U.G. SCH. 40 PVC WITH (2) #8 CU THIN-2 AND (1) #10 GND. CONDUIT AND CONDUCTOR INSTALLATION PER SCE STANDARDS
 - (H) GATE CONTROLLER AND CHAMBERLAIN ELITE SL3000 1/2 HP MOTOR TO BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS
 - (I) 200A, 120/240V, SINGLE PHASE TRAILER PANEL "D". SCE TO FURNISH AND INSTALL WEATHERHEAD FOR OVERHEAD SERVICE ENTRANCE
 - (J) FURNISH AND INSTALL 120/240V, 400 AMP SERVICE CT METER AND DISTRIBUTION PANEL "B" PER SCE ESR 2-32 & 2-33 STANDARDS. SEE SINGLE LINE FOR BREAKERS
- PROPOSED SERVICE CT METER AND DISTRIBUTION PANEL
- FUTURE TRAILER DISTRIBUTION PANEL
- 1/2" X 8' COOPER CLAD GROUND ROD, ERICCO OR APPROVED EQUAL
- BURIED BARE COPPER GROUND WIRE
- SERVICE CONDUCTOR, SEE SINGLE LINE

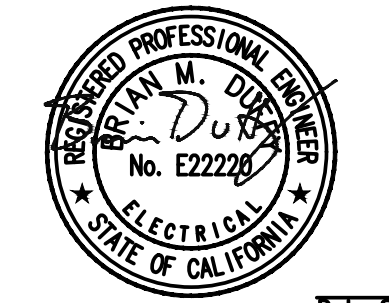
GENERAL NOTES

- THE REQUIREMENTS AND INFORMATION SET OUT BELOW ARE PROVIDED FOR THE CONTRACTOR'S CONVENIENCE AND DO NOT ENCOMPASS ALL PROJECT REQUIREMENTS DESCRIBED BY THE PROJECT PLANS AND SPECIFICATIONS AND/OR APPLICABLE LAWS, REGULATIONS AND/OR BUILDING CODES.
- ELECTRICAL UTILITY LINE TO BE PROTECTED IN PLACE WHEN POSSIBLE. IF ANY CONSTRUCTION CONSTRAINTS ARE ENCOUNTERED DURING THE INSTALLATION OF THE ELECTRICAL EQUIPMENT REMOVE AND RELOCATE THE ELECTRICAL LINE AS NEEDED. IF THE ELECTRICAL LINE CANNOT BE SALVAGED THE CONTRACTOR MUST LAWFULLY DISPOSE OF THE ELECTRICAL LINE AND REPLACE WITH LIKE AND KIND.
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 - SEE SINGLE LINE DIAGRAM ON SHEET E1.1.
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SINGLE LINE DIAGRAM



PLANS PREPARED BY:
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 Tel: (509) 528-4400
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Date Signed:

LOCATION: SHAVER LAKE, CALIFORNIA	SHEET NO. E1.1
SHAVER LAKE SERVICE CENTER LAYDOWN YARD PARTIAL ELECTRICAL SITE PLAN	SCALE: 1" = 20'
EDISON An EDISON INTERNATIONAL Company	11 OF 11 SHTS.

Drawing: P:\218-00005\Project\Drawings\118000_0102.dwg; E1.1 Partial Electrical Site Plan - BCF.ctb
 Plot by: gperkins Aug 31, 2013 - 11:10am





NO
PARKING
ANY
TIME

VISITOR
PARKING

41694





05.18.2018



05.18.2018



05.18.2018



05.18.2018



05.18.2018



05.18.2018









